



**PLANNING COMMISSION**  
**Tuesday, September 10, 2019**  
265 Strand Street, St. Helens, OR 97051  
[www.ci.st-helens.or.us](http://www.ci.st-helens.or.us)

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**Welcome!**

1. **7:00 p.m. - Call to Order and Flag Salute**
2. **Consent Agenda: Approval of Minutes**
  - 2.A. Planning Commission Minutes dated August 13, 2019  
[081319 PC Minutes DRAFT](#)
3. **Topics from the Floor: Limited to 5 minutes per topic (not on public hearing agenda)**
4. **Public Hearings (times reflect earliest start time)**
  - 4.A. 7:00 p.m. - Variance (Fence Height) at 55 DuBois Lane - Vaerewyck  
[V.16.19 Staff Report](#)
  - 4.B. 7:30 p.m. - Conditional Use Permit at property adjacent to US 30 & Gable Road - Community Development Partners  
[CUP.3.19 Staff Report](#)
5. **Acceptance Agenda: Planning Administrator Site Design Review -**
  - a. *Site Design Review (Minor) at 454 Milton Way - Modifications to an existing telecommunications tower*
6. **Planning Director Decisions -**
  - a. *Sign Permit at 1934 Columbia Blvd. - New wall sign on an existing business*
  - b. *Sign Permit (Banner) at 2100 Block of Columbia Blvd. - Halloween Parade*
  - c. *Lot Line Adjustment at 58625 & 58645 McNulty Way - Semling Construction Center, LLC*
7. **Planning Department Activity Report**

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7.A. Planning Department Report dated August 28, 2019  
[2019 AUG Planning Dept Rept](#)

8. **For Your Information Items**

9. **Next Regular Meeting - October 8, 2019**

10. **Adjournment**

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# City of St. Helens Planning Commission

Draft Minutes

August 13, 2019

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**Members Present:** Commissioner Cohen  
Commissioner Lawrence  
Commissioner Semling  
Commissioner Webster  
Chair Hubbard  
Vice Chair Cary

**Members Absent:** Commissioner Stenberg

**Staff Present:** Councilor Carlson  
City Planner Graichen  
Associate Planner Dimsho

**Others:** Craig Allison  
David Fix  
Mike Mangold  
Pam Rensch  
Molly Matchack

- 1) **7 p.m. - Call to Order and Flag Salute**
- 2) **Consent Agenda: Approval of Minutes**  
2.A Planning Commission Minutes dated July 9, 2019

**Motion:** Upon Commissioner Semling's motion and Commissioner Lawrence's second, the Planning Commission unanimously approved the Planning Commission Minutes dated July 9, 2019. Commissioner Cohen and Commissioner Webster did not vote due to their absences from that meeting. [Ayes: Commissioner Lawrence, Commissioner Semling, Vice Chair Cary; Nays: None]

- 3) **Topics from the Floor: Limited to 5 minutes per topic (not on public hearing agenda)**

There were no comments.

- 4) **Public Hearings (times reflect earliest start time)**  
4.A 7:00 p.m. - Variance at 214 N. 17th Street - Rensch Construction & Properties, Inc.

Chair Hubbard opened the Public Hearing at 7:01 p.m. There were no ex-parte contacts, conflicts of interests, or bias in this matter. City Planner Jacob Graichen entered the staff report dated August 2, 2019 into the record. Graichen described the proposal and recommended conditions of approval, as presented in the staff report. He said this variance request is the



result of an unexpected storm line being discovered late in the process of a single-family dwelling building permit review. The applicant had already developed their housing plans, so in order for the same building plans to fit on the site, they are requesting a front yard (setback) variance. Commissioner Semling asked if the storm line functions. Graichen said yes, and noted the City Engineer's comments in the staff report.

### **In Favor**

**Rensch, Pam. Applicant.** Rensch said when they originally called for utility locates, the storm line was not identified. They called for locates again when the building permit was closer to approval. That was when the storm line was discovered. They would not have spent the money on these plans if they had seen the storm line earlier in the process. There was no easement on record when she went to the title company.

### **In Opposition**

No one spoke in opposition.

### **End of Oral Testimony**

There were no requests to continue the hearing or leave the record open.

### **Close of Public Hearing & Record**

The applicant waived the opportunity to submit final written argument after the close of the record.

### **Deliberations**

Commissioner Lawrence asked if siting the building in its proposed location could be a hazard to public health or safety. Graichen said it is his job to be paranoid. He does not think this proposal threatens public safety. Commissioner Cohen said it seems like it was no one's intended fault.

**Motion:** Upon Commissioner Cohen's motion and Commissioner Webster's second, the Planning Commission unanimously approved the Variance Permit at 214 N. 17th Street as presented. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

**Motion:** Upon Commissioner Cohen's motion and Commissioner Webster's second, the Planning Commission unanimously approved the Chair to sign the Findings & Conclusions once prepared. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

### **4.B        7:45 p.m. - Lot Line Adjustment at 58144 Old Portland Road - Port of Columbia County**

Chair Hubbard opened the Public Hearing at 7:46 p.m. There were no ex-parte contacts, conflicts of interests, or bias in this matter. Graichen entered the staff report dated August 2, 2019 into the record. Graichen said there was vague dialogue leading up to the application, and



aspects of the application remained unclear. He showed the Commission the original submitted Lot Line Adjustment. This strange alignment was partly why he decided to send the decision to the Commission. Graichen said that in order for the Port of Columbia County to receive the financing they need, the state is requiring that they have the newest building on its own lot. Vice Chair Cary asked where the financing is from. Graichen said it is from Business Oregon (part of the Infrastructure Finance Authority).

Commissioner Cohen confirmed what allows them to do a lot line adjustment versus a partition. Graichen noted the two original lots of record are included in the attachments. Commissioner Cohen asked what would happen if the Port tried to sell the island parcel. Graichen said the purpose of land division is for transfer of ownership. Staff is recommending the conditions included in the staff report because his obligation is to view the proposal as if they were trying to sell the island parcel. Industrial zoned property is valuable from a tax base and from an employment standpoint, so we do not want to end up with a messy parcel. Commissioner Cohen asked if the buildings met required setbacks. Graichen said industrial setbacks are based on proximity to other zones. Building code setbacks have been addressed with a condition. Chair Hubbard asked what if Columbia River PUD wanted something different than the easements proposed. Graichen said some assumptions have to be made about existing and future uses. Since the island parcel is pretty built out, we can make some accurate assumptions about future use. Graichen said he has structured the recommended easements to be wide enough to accommodate access and utilities. Graichen also noted some recommended conditions that required changes to the proposed lot line to accommodate existing landscaping and parking.

### **In Favor**

**Allison, Craig. Applicant.** Allison is the Port of Columbia County's Operations Manager and Deputy Director. He has been in this position for about ten years. He is the developer, architect, and executor of the Multnomah Industrial Park. When he came on board in 2010, the site was vastly underutilized. In 2008, the biggest building on the site collapsed. ORPET replaced that building. Since then, they've been replacing, renovating, and adding buildings. Scott Jensen has been the primary planner with the Port for this Lot Line Adjustment. The Port's reason for doing the Lot Line Adjustment is to utilize the state's financing program. The state also financed, in part, renovations to Building B, which was roughly \$1.6 million. Last year, they expanded with a new \$1.1 million facility (Building E). They are servicing this debt. Business Oregon has had transitions in the last year, and these changes necessitated new conditions to their lending practices. For the Building E financing, a new bonding requirement was sprung on the Port for the parcels related to the project's financing. This Lot Line Adjustment is to satisfy the requirements of the state to complete the loan process. The strange parcel dimensions Graichen mentioned initially reflected the lease hold. He can understand Graichen's conditions to tweak the boundaries to accommodate parking and landscaping. The Port has no intention to sell. They have always wanted to develop, maintain, and rent this industrial park. They are close to achieving the final vision, except for this lending stumbling block. He would like to ask that the easements be kept to a minimum. The process of easement creation takes time and money. He would ask that this process is streamlined. November, which is the deadline for them to get the financing, will come very quickly. Allison said one difficulty of adjusting the lot line to include the parking to the northeast of Building E is that then they need an additional access easement with themselves. Graichen said the parking needs to be included in the property, since it is associated with the development.



Allison asked about a blanket easement between the two properties. This would make the process a lot simpler and faster. Graichen said our legal counsel has recommended against utilizing blanket easements when the location of utilities and access is known. Allison said with a blanket easement, they would be codifying what occurs on the property today. Allison said it could be a reciprocal blanket access and utility easement. Graichen said the conditions, as written, do not explicitly deny blanket easements. Graichen said a shared road maintenance agreement is also required. Allison said for common areas and roads, the Port is moving towards adopting fee assessments to charge their tenants on a lease. Graichen said leases are not tied to the land. Allison proposed requiring a joint maintenance agreement at the time of the sale of the property. Graichen said he thought of that, but is not entirely comfortable with delaying the requirement until the property sells. However, he noted the conditions do not explicitly prohibit that method. Any proposal will be subject to review.

### **In Opposition**

No one spoke in opposition.

### **End of Oral Testimony**

There were no requests to continue the hearing or leave the record open.

### **Close of Public Hearing & Record**

The applicant waived the opportunity to submit final written argument after the close of the record.

### **Deliberations**

Commissioner Cohen said this Lot Line Adjustment is complicated because it is already developed. He thinks staff adequately addressed the issues. The easements are necessary to address potential disparate ownership. Commissioner Lawrence agreed.

**Motion:** Upon Commissioner Cohen's motion and Commissioner Webster's second, the Planning Commission unanimously approved the Lot Line Adjustment at 58144 Old Portland Road as presented. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

**Motion:** Upon Commissioner Cohen's motion and Vice Chair Cary's second, the Planning Commission unanimously approved the Chair to sign the Findings & Conclusions once prepared. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

### **5) Discussion Items**

#### **5.A Riverfront District Architectural Guidelines Recommendation - New Sign at 290 S. 1st Street**

Associate Planner Dimsho introduced the Commission to the proposal, as presented in the memo. The applicant is proposing a new neon projecting sign adjacent to the Plymouth Pub. The Commission is to make a recommendation to staff for compliance with the Riverfront District Architectural Guidelines.



**Molly Matchack, Applicant.** Matchack said she is opening Molly's Market at this location. Commissioner Webster asked if their sign was a similar size to the Plymouth Pub sign. She said she thinks it is smaller. Chair Hubbard asked if there is another suite to be located between the pub and the market. Plymouth Pub will be occupying the suite in between the new market and pub. Chair Hubbard asked if the design was hers. Matchack said her son designed it. Vice Chair Cary asked what Molly's Market will carry. Matchack said it will be packaged food to-go, like cheese, crackers, and bread. Chair Hubbard clarified the amount of neon on the sign. Commissioner Cohen asked if it blinks. Matchack said no.

**Motion:** Upon Commissioner Cohen's motion and Vice Chair Cary's second, the Planning Commission unanimously recommended approval of the Sign Permit at 290 S. 1st Street as presented. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

#### 5.B Certified Local Government - Historic Preservation Grant Scoring

Commissioner Cohen asked why we score side facades. Dimsho said the idea is that we score projects with front-facing façade work higher than just side-facing façade work. Graichen said sometimes side façade work can keep the entire historic structure in good shape. Dimsho said even though she is not in attendance tonight, she received Commissioner Stenberg's scores via email earlier today. Commissioner Cohen asked about the financial resources category. Dimsho said the Columbia Theatre is using another grant through the state to match this one. She confirmed with the state that matching a grant with another grant is acceptable.

After tallying the scores from the Commission, Dimsho said the Columbia Theatre was the highest scoring applicant. Dimsho said she will work with the State Historic Preservation Office to get an approved work plan for the theater. If something does not work with their work plan, the second place applicant, Elliot Michael, will be next in line for receiving the funds.

#### 6) Acceptance Agenda: Planning Administrator Site Design Review

**Motion:** Upon Commissioner Cohen's motion and Commissioner Webster's second, the Planning Commission unanimously approved the Acceptance Agenda: Planning Administrator Site Design Review. [Ayes: Commissioner Cohen, Commissioner Lawrence, Commissioner Semling, Commissioner Webster, Vice Chair Cary; Nays: None]

#### 7) Planning Director Decisions

There were no comments.

#### 8) Planning Department Activity Report

##### 8.A Planning Department Activity Report dated July 29, 2019

There were no comments.

#### 9) For Your Information Items

There were no comments.

#### 10) Next Regular Meeting - September 10, 2019



11) **Adjournment**

*There being no further business before the Planning Commission, the meeting was adjourned at 8:53 p.m.*

*Respectfully submitted,*

*Jennifer Dimsho  
Associate Planner*



**CITY OF ST. HELENS PLANNING DEPARTMENT  
STAFF REPORT  
Variance V.16.19**

**DATE:** September 3, 2019  
**TO:** Planning Commission  
**FROM:** Jacob A. Graichen, AICP, City Planner  
 Jennifer Dimsho, Associate Planner

**APPLICANT:** Brian & Shannon Vaerewyck  
**OWNER:** Same

**ZONING:** General Residential, R5  
**LOCATION:** 55 DuBois Lane; 4N1W-4CB-9300, 9399, & 9400  
**PROPOSAL:** Variance for fence height (8 feet in front and rear yards)

The 120-day rule (ORS 227.178) for final action for this land use decision is **December 6, 2019**.

**SITE INFORMATION / BACKGROUND**

The property is developed with a detached single-family dwelling. They constructed a building addition (garage) in 2019. The rear portion of the subject property is within Milton Creek. Approximately half of the site is in an “area of special flood hazard” (100-year floodplain), and a slightly smaller section of the property is in the floodway. Due to the geometry of DuBois Lane, the property has two abutting neighbors (65 and 77 DuBois Lane) along the property line of the proposed fence. Applicant proposed to build an 8 foot tall fence (2 foot concrete footing and 6 foot cedar fence) along the southeast property line from the front of the property to the existing retaining wall in the back.



*Subject property from DuBois Lane (left). Survey pin located near rebar and string (below).*





## PUBLIC HEARING & NOTICE

Hearing dates are as follows:

September 10, 2019 before the Planning Commission

Notice of this proposal was sent to surrounding property owners within 100 feet of the subject property on August 20, 2019 via first class mail. Notice was sent to agencies by mail or e-mail on the same date. Notice was published in The Chronicle on August 28, 2019.

## AGENCY REFERRALS & COMMENTS

**City Public Works** - There is an existing storm drain pipe that extends from the catch basin adjacent to the new concrete driveway and discharges to Milton Creek. The pipe runs down the property line next to the new garage structure that has been recently constructed at 55 Dubois Lane. There is some concern from Public Works maintenance staff that the proposed concrete “base” for the fence could potentially interfere with access to the storm pipe if there were a need to excavate and repair the line in the future. Maintenance equipment will need to access the storm pipe through the catch basin adjacent to the driveway and therefore no fence, base, or structure should be constructed within 4 feet of the southerly edge of the catch basin. Although the concrete base that is proposed for the fence is not ideal for future access to the storm pipe if necessary, we are not recommending against its construction. However, to maintain the critical access through the catch basin and ensure that cleaning and maintenance equipment have enough room to operate, it is highly recommended that no fence or structure be constructed within 4 feet of the existing catch basin as shown in the attached image.

## APPLICABLE CRITERIA, ANALYSIS & FINDINGS

### SHMC 17.72.090 Setbacks for fences or walls

[...]

(2) Fences or walls:

- (a) May not exceed four feet in height in a required front yard along local or collector streets or six feet in all other yards and, in all other cases, shall meet vision clearance area requirements (Chapter 17.76 SHMC);
- (b) Are permitted up to six feet in height in front yards adjacent to any designated arterial or street. For any fence over three feet in height in the required front yard area, permission shall be subject to review of the location of the fence or wall;
- (c) All fences or walls shall meet vision clearance area requirements (Chapter 17.76 SHMC);
- (d) All fences or walls greater than six feet in height shall be subject to building official approval.

### FINDINGS

- 2) **(a)** DuBois Lane is a local street, which means the fence allowed without a variance is four feet in the front yard and six feet in all other yards.
- (b)** This section is not relevant.
- (c)** The proposed fence is not within a vision clearance area.



**(d)** This shall be a condition of approval.

**SHMC 17.108.050 (1) – Criteria for granting a Variance**

- (a) The proposed variance will not be significantly detrimental in its consequence to the overall purposes of this code, be in conflict with the applicable policies of the comprehensive plan, to any other applicable policies and standards of this code, and be significantly detrimental in its consequence to other properties in the same zoning district or vicinity;
- (b) There are special circumstances that exist which are peculiar to the lot size or shape, topography or other circumstances over which the applicant has no control, and which are not applicable to other properties in the same zoning district;
- (c) The use proposed will be the same as permitted under this code and city standards will be maintained to the greatest extent that is reasonably possible while permitting some economic use of the land;
- (d) Existing physical and natural systems, such as but not limited to traffic, drainage, dramatic landforms, or parks, will not be adversely affected any more than would occur if the development were located as specified in the code; and
- (e) The hardship is not self-imposed and the variance requested is the minimum variance which would alleviate the hardship.

The Commission needs to find all these criteria **(a)** – **(e)** are met in order to approve the variance

**FINDINGS**

**(a)** This criterion requires a finding that the variance will not be detrimental.

- See applicant's narrative.
- Staff comment: The fence will be visible from DuBois Lane. If the Commission feels the fence has a detrimental impact to the surrounding neighborhood, one way to reduce the visibility of the fence would be to limit the variance request to the rear yard only. This could reduce visibility of the fence from the street, and due to the geometry of DuBois Lane, it would reduce the number of properties abutting the fence from two properties to one property.

The floodplain and floodway are factors to this criterion. The subject property is partially within an "area of special flood hazard," more commonly known as the 100 year floodplain as identified by the flood insurance rate map (FIRM) 41009C0452D dated November 26, 2010. This floodplain is associated with Milton Creek.

The floodway (within the "area of special flood hazard") is the heart and most destructive portion of a floodplain area. The floodway includes the channel of a stream and the overbank areas.

The FIRM identifies the Milton Creek floodway within the subject property and the area of the proposal. The floodway is important as it is the area to discharge and store floodwaters or flows during flood events. Flood waters do not acknowledge property



lines; we must review flood water function as a system that crosses many property lines. The system needs to work for flood management for all, not just a single property.

As such, Chapter 17.46 SHMC has a requirement to:

Prohibit encroachments, including fill, new construction, substantial improvements, and other development unless certification by a registered professional civil engineer is provided demonstrating through hydrologic and hydraulic analysis performed in accordance with standard engineering practice that the proposed encroachments shall not result in any increase in flood levels during the occurrence of the base flood discharge.

This is more commonly known as a “no-rise” certification.

Adding encroachments within the floodway are likely to increase flood levels elsewhere, especially in a smaller stream such as Milton Creek.

Reviewing the FIRM and observing the site in the field, there are retaining walls along the creek for the properties along DuBois Lane (between the subject property and Milton Way). These retaining walls are within the floodway in all cases, including the subject property.

Staff’s assumption is that the floodway extends to the back side of a shed in the southwest corner of the developed site. No fill, new fence, concrete foundation for the fence, increased retaining wall height, or any other improvement shall extend any further than the back side of the shed.



*The rear of the developed portion of the subject property facing downstream.*

*The back side of the shed is the assumed floodway line. This Variance shall not allow anything beyond the this side of the shed to ensure floodwaters are not displaced to other properties.*

**(b) The criterion requires a finding that there are special and unique circumstances.**

- See applicant’s narrative.
- Staff comment: The Commission needs to decide if the applicant complies with this criteria.



**(c) This criterion prohibits a use variance and requires a finding that the applicable standards are maintained to the greatest extent that is reasonably possible.**

- See applicant's narrative.
- Staff comments: The applicant is not proposing an exception to vision clearance standards.
- This request is for an 8 foot fence (2 foot concrete footing, 6 foot cedar fence) in both the front and rear yards. Limiting the variance request to the rear yard would help maintain the standards to a greater extent because the request would be for a variance from 6 feet to 8 feet, instead of the 4 feet to 8 feet request in the front yard.

**(d) This criterion requires a finding that existing physical and natural systems will not be adversely affected as a result of the requested Variance.**

- See applicant's narrative.
- Staff comments: Given Public Works' comments regarding the storm line and catch basin, the applicant shall not place any concrete structure within four feet of the catch basin (as shown on the attachment). This will ensure that cleaning and maintenance equipment have enough room to operate in and around the catch basin.

New encroachments into the floodway would impact other properties by displacing floodwaters during a flood event. A privacy fence and other improvements are not justified for this potential wide spread impact. No improvements shall be allowed past the back side of the existing shed.



*Existing retaining wall along Milton Creek.*

*Back side of the shed is the approximate floodway boundary. The retaining wall is within the floodway and below the base flood elevation.*

*The retaining wall was likely built before the City had floodplain rules (i.e., pre 1987).*



**(e) This criterion requires a finding that the variance issue is not self-imposed and that the variance is the minimum necessary to alleviate the hardship.**

- See applicant's narrative.
- Staff comment: The hardship has not be adequately defined in staff's opinion. The Commission needs to decide if the applicant complies with this criteria.

### CONCLUSION & RECOMMENDATION

**Based upon the facts and findings herein, staff recommends approval of this Variance with the following conditions:**

1. This Variance approval is valid for a limited time pursuant to SHMC 17.108.040.
2. The proposed fence shall be subject to Building Department approval.
3. No concrete structure, including the proposed two foot concrete footing, shall be located within four feet of the storm water catch basin in the front yard (as pictured).
4. This Variance does not allow any improvements or fill within the floodway. Nothing shall occur / be allowed beyond the side of the existing shed facing Milton Creek.
5. **If the Planning Commission wants to include limitations on the extent of the variance, that could go here. For example:** this Variance shall not be allowed in the front yard (i.e., first 20' back from DuBois Lane). **Also consider the max height allowed by the Variance.**
6. Prior to any development or building permit issuance, the applicant shall provide a revised plan demonstrating compliance with conditions 3 and 4 above. **Depending on the Commission's decision in regards to condition 5 or others, other conditions may need to be listed here. For example "conditions 3-5" instead of "3 and 4."**
7. Prior to any land clearing, alteration, or physical construction (other than survey work or environmental testing) on the subject property(ies), the property owner and developer, if any, shall obtained all required federal, state, and local authorizations, permits and approvals for the proposed development, including any proposed use, or alteration of the site, including also any off-site improvements. Owner/developer shall be solely responsible for obtaining all approvals, permits, licenses, insurance, and authorizations from the responsible federal, state and local authorities, or other entities, necessary to use the property in the manner contemplated, including all authorizations necessary to perform land clearing, construction and improvement of property in the location and manner contemplated. This provision includes, specifically, a permit or statement from the National Marine Fisheries Service and/or Fish and Wildlife Service that owner's proposed use and/or development will not take or harm any endangered or threatened species as that term is defined in applicable federal statutes and administrative rules. The City of St. Helens has no duty, responsibility or liability for requesting, obtaining, ensuring, or verifying owner/developer's compliance with the applicable state and federal agency permit or approval requirements. Any permit or authorization granted by the city, including any exemption, exception, permit, approval or



variance pursuant to the Community Development Code (SHMC Title 17) shall not in any way be interpreted as a waiver, modification, or grant of any state or federal agency permits or authorizations or permission to violate any state or federal law or regulation.

Owner/developer shall be held strictly liable, and shall hold the city of St. Helens harmless for administrative, civil and criminal penalties for any violation of federal and state statutes, including but not limited to the Clean Water Act, Endangered Species Act and regulations implementing such laws.

8. Owner/applicant and their successors are still responsible to comply with the City Development Code (SHMC Title 17), except for the Variance granted herein.

**Attachments:** *Site Plan, Fence Elevation, Public Works diagram, Applicant's Narrative*

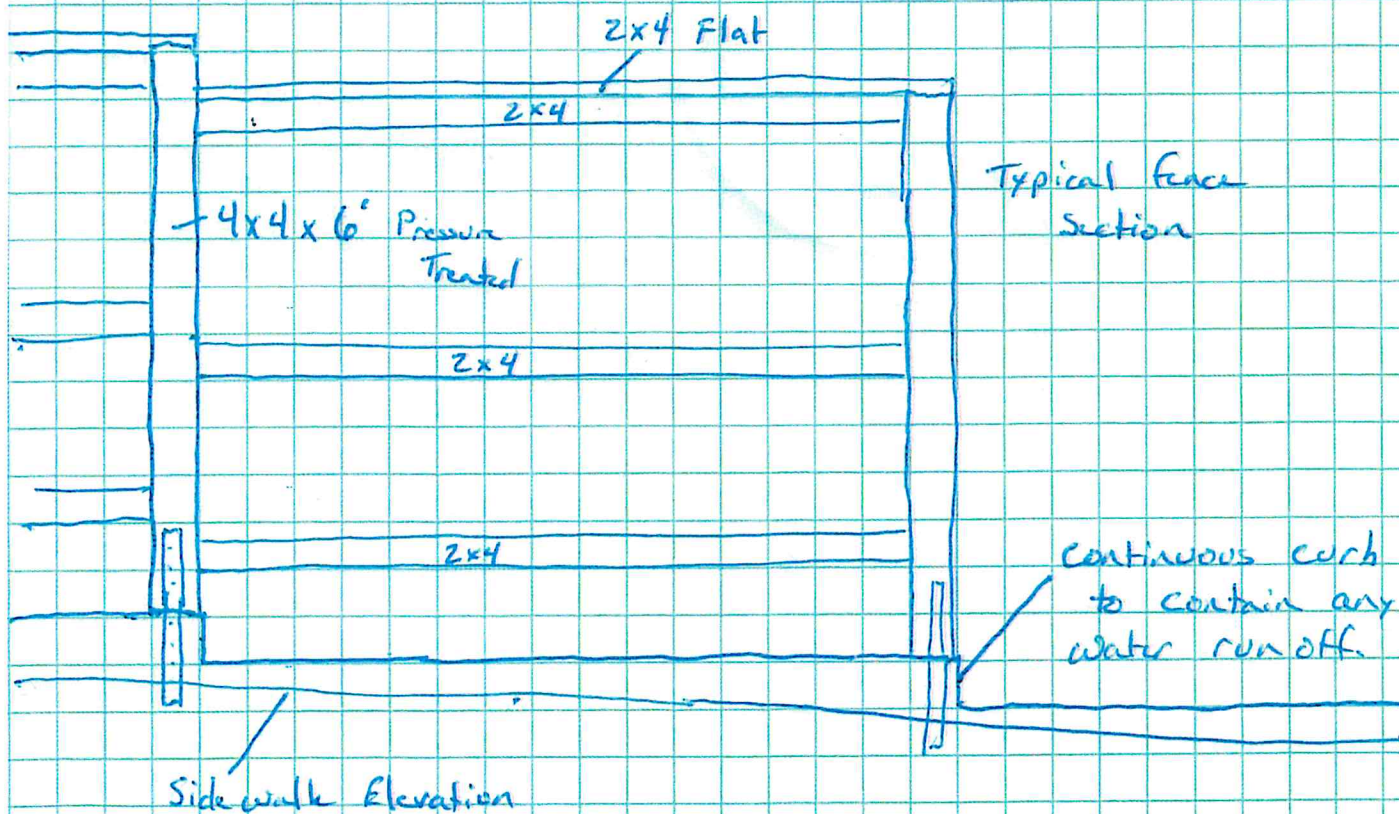
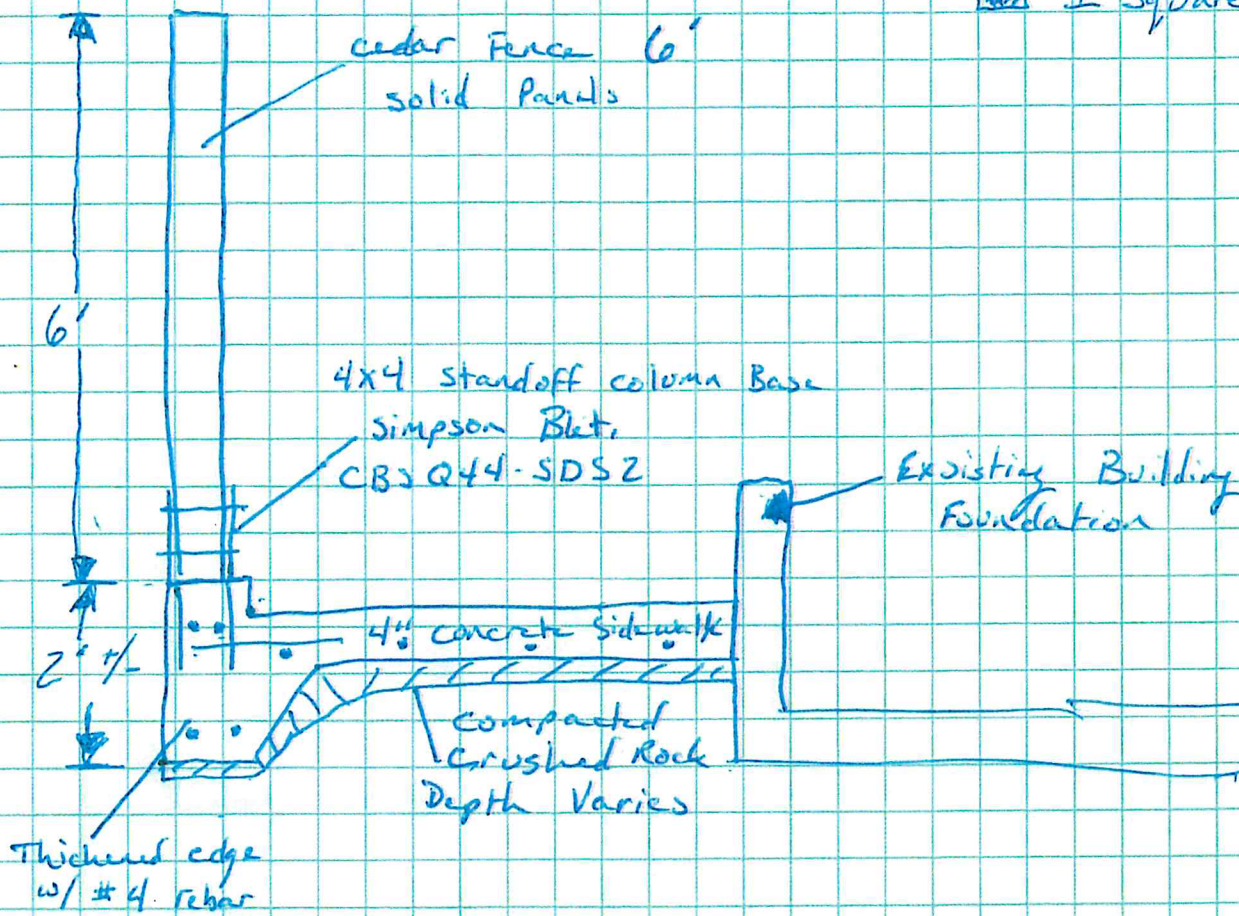


# 55 W Boies Lane Variance Site Plan





1 Square = 6"





V.16.19 Public Works Catch Basin Diagram





Description of the Land Use Request:

From the NE corner to the SE corner of the property we wish to have a continuous 6ft cedar fence on a (+/-) 2ft concrete footing on a staggered slope. This would also raise the seawall in the back of the property and allow clean fill to be placed to maintain the grade of the property.

The requested 8ft fence comprising of 6ft Cedar and 2ft concrete footing helps better privacy and will not be significantly detrimental in its consequence to the overall purposes of this code, be in conflict with the applicable policies of the comprehensive plan, to any other applicable policies and standards of this code, and be significantly detrimental in its consequence to other properties in the same zoning district or vicinity

The property is in the flood plain and has an existing seawall to help maintain the flood damage and the natural erosion caused by the creek. We do not have control over the flow and direction of the creek.

The use of the fence will be the same as permitted under this code and city standards will be maintained to the greatest extent that is reasonably possible while permitting some economic use of the land

The use of the fence and downward grade of the footing will not be adversely affected any more than would occur if the development were located as specified in the code. We believe that it will enhance the use and prevent damage to the property and surrounding property.

The construction of the footing and fence is not self-imposed and the variance requested is the minimum variance which would alleviate the hardship.



**CITY OF ST. HELENS PLANNING DEPARTMENT  
STAFF REPORT  
Conditional Use Permit CUP.3.19**

**DATE:** September 3, 2019  
**TO:** Planning Commission  
**FROM:** Jacob A. Graichen, AICP, City Planner

**APPLICANT:** Community Development Partners  
**OWNER:** Gable Road, LLC (4N1W-8AA-200)  
 Elliot H. Fixler (4N1W-8AA-301)

**ZONING:** General Commercial, GC  
**LOCATION:** Property located adjacent to US30 and Gable Road, but excluding lands at the intersection of US30/Gable Road  
**PROPOSAL:** 238 unit multidwelling (apartment) complex

**The 120-day rule (ORS 227.178) for final action for this land use decision is December 24, 2019.**

**SITE INFORMATION / BACKGROUND**

Property currently identified as 4N1W-8AA-301 (2285 Gable Road) is majority of the former location of the Columbia County transfer station (waste management). Little remains from that development except some fencing and paved and gravel surfaces. Note that this address will need to change as 2285 is the only number on the north side of this segment of Gable Road with an odd number.

Property currently identified as 4N1W-8AA-200, does not appear to have previous development except for a portion that was part of the former Columbia County transfer station (waste management). A substantial portion of this property includes wetlands and drainageways. Trees are concentrated on the east side of the site, predominately within forested wetlands.

**PUBLIC HEARING & NOTICE**

Hearing dates are as follows: September 10, 2019 before the Planning Commission.

Notice of this proposal was sent to surrounding property owners within 300 feet of the subject property(ies) on August 21, 2019 via first class mail. Notice was sent to agencies by mail or e-mail on the same date. Notice was published in the The Chronicle on August 28, 2019.

**AGENCY REFERRALS & COMMENTS**

As of the date of this staff report, the following agency referrals/comments have been received that are pertinent to the analysis of this proposal:



**Comcast Cable Communications, Inc.:** I wanted to pass along info to the future developer in regards to the frontage on Gable Road. If Comcast has to relocate our aerial facilities along the frontage due to private development, in most cases this cost is passed to the developer for reimbursement.

**Fire Marshall:** *Per discussion on August 26, 2019:* 1) Vegetation/landscaping between drive aisles and buildings should not be greater than 10 feet at maturity so as to not interfere with ladder truck access to upper floors. 2) Need a 26' wide road next to fire hydrants that extends 20' in each direction. Also, where buildings exceed 30 in height the access road to them needs to be at least 26' in width. 3) The parking lot just west of buildings 1 and 4 should have an emergency access to Gable Road with removable bollards.

*Staff response: 1) will be a condition of approval. 2) most drive aisles are 26' in width where there is adjacent parking. 3) will be a condition of approval.*

**City Engineering/Public Works:** Traffic and Streets: Huge potential for overwhelming the Gable/Highway 30 intersection. During high volume times, traffic already backs up past both of the Walmart entrances, and can easily be back to the railroad spur crossing at Railroad Ave. when there is a train coming through on the main track at the intersection. It would be recommended that frontage improvements be constructed with the soon-to-be adopted Riverfront Connector Plan and that a median/turn lane be included along the entire frontage. There is existing street lighting on Gable Road along the property frontage but it does not appear that the existing lighting will be located adjacent to the proposed entrance/exit locations. The street lighting shall be analyzed to determine if the existing lighting will provide adequate illumination for the entry/exit intersections.

Water: There is a 16-inch public water main on the south side of Gable that has capacity for the development. This 16-in main shall be extended southeasterly along the Gable Road ROW to a point that is a minimum of half way across the entire proposed development frontage. All public watermain that are constructed through the development property shall be located within a public utility easement and shall be in locations that are easily accessible for maintenance and repair. Public water mains shall be constructed of ductile iron pipe per the St. Helens Construction Standards. Ideally, the public water system would loop through the project and reconnect to the public main in Gable Road for better water service to the project. There shall be a blow off installed at the end of any main that does not loop/reconnect to the public system. A water sampling station may be required to be installed. Backflow devices shall be installed per Public Works requirements. All water meters shall be Meter Meter radio read meters; prior approval by Public Works of proposed meter models will be required. Public fire hydrants shall be approved by the Fire Marshal.

Sanitary Sewer: There is an existing sanitary sewer system located on the south side of Gable Road that runs along the majority of the frontage of the subject property. This has the capacity to serve this proposed development. All public sanitary sewer mains and structures that are constructed through the development property shall be located within a public utility easement and shall be in locations that are easily accessible for maintenance and repair. Separation between the public water and public sanitary sewer mains shall meet all health and construction standards for separation and materials. If trash enclosures are to be covered and include a drain,



the drain shall be connected to the sanitary sewer and shall be constructed in such a way that rainwater runoff is not captured by these drains.

**Storm Drain:** There are several wetlands located throughout the project site. There are also at least 2 known existing storm drains that discharge on the westerly side of the property that carry water from the highway and from further west. These are significant drainage systems, especially the system located near the northwest corner of the project site and shall be maintained in such a way that the historical drainage will not be negatively impacted and preferably improved. The discharge of the system located at the northwest corner of the site has been partially blocked in the past, creating fairly significant flooding on the west side of the highway so it is imperative that this development does not compound this issue and maintains or improves the flow characteristics. Any storm drains and structures that are proposed to be public that will be located on the property shall be within a public easement of adequate width and shall be in locations that are easily accessible for maintenance and repair. The significant wetland on the east side of the project can be used to discharge new runoff from the apartments and surrounding parking areas, etc. although it is most likely that enhancements to the wetlands and detention will be required to mitigate the additional flows generated from the development. A structure or method of providing nominal cleaning of the stormwater collected by any new system, private or public, shall be incorporated into the public and/or private storm drainage system(s). This can include measures such as, but not limited to, sumps in catch basins, stormwater quality manholes located before the discharge point, “stormceptor” style manholes, etc. The existing culvert crossing Gable Road is undocumented (there are no plans on file or any other documentation on this pipe) and may have to be repaired/improved/replaced. Ultimately, the storm water from the site crosses Gable Road and drains to McNulty Creek, which is known to be “flashy” – quick to fill during storm events and cause localized flooding along its path in heavier events. An in-depth stormwater analysis shall be conducted to determine the impacts of the development on the downstream system. There is a complete lack of a formal storm drainage system along the property frontage on Gable Road and storm drainage shall be included with the roadway frontage improvements.

### APPLICABLE CRITERIA, ANALYSIS & FINDINGS

**Deemed Complete Info:** This application was originally received on June 28, 2019. Staff identified missing information or other aspects that rendered the application incomplete and notified the applicant of the issue pursuant to SHMC 17.24.050 on July 23, 2019. The applicant provided revised or new information and paid the application fee; the application was deemed complete on August 26, 2019.

\* \* \*

**Permitting History:** The two properties were annexed in 2000 (Ord. No. 2823) and 2004 (Ord. No. 2917) as Light Industrial zoning. The zoning was changed for both properties around 2006 to General Commercial (file ZA.2.06).

The shared property line between the two properties was adjusted to the current configuration around 2007 (file LLA.1.07).



\* \* \*

**Zoning Compliance:** The site is zoned General Commercial, GC. Multidwelling units are a conditionally permitted use, thus this Conditional Use Permit. When multidwelling units are proposed in the GC zone, the standards of the Apartment Residential, AR zone apply.

The two subject properties together are about 15.91 acres or 693,039.6 square feet. The density calculation is 5,000 s.f. for the first two units and 1,500 s.f. for each unit thereafter. The 238 proposed units fits within this calculation.

Minimum 20' yards (setback) are required along Gable Road; plans comply.

Minimum 10' yard (setback) is required along the property lines that don't abut a public street; plans comply.

Maximum building height is 35 feet. The applicant proposes four principal building types. Their proposed heights are:

- Townhouse building—28 feet from ground to roof peak.
- Family Housing building—39 feet from ground to roof peak.
- Senior Housing building—33 feet from ground to roof peak.
- Commons / Community Building—27' from ground to roof peak

Three of the four clearly comply. **The Family Housing type needs to be examined further.** The Development Code defined "building height" as:

"Building height" means the vertical distance of a building measured from the average elevation of the finished grade within 20 feet of the structure to the highest point of the roof.

Then SHMC 17.68.010 reads:

Projections such as chimneys, spires, domes, elevator shaft housings, towers excluding TV dish receivers, aerials, flag poles, and other similar objects not used for human occupancy are not considered buildings.

Plans show the 35' mark within the attic space, suggesting that the roof area above the third floor will be non-occupiable attic area.

**How does the Commission feel about this? We have a few choices:**

1. Allow as meeting intent of code—which may include how easy it would be (no not) to covert the attic to living space, but is attic space a "similar object" per the definition?;
2. Allow as a condition of the Conditional Use Permit;
3. Roofline must comply by reducing height or a Variance may be applied for.

See item #1 in the attached [MWA Architects Memorandum dated and received August 14, 2019](#) in regards to this height issue. **Imagine if this argument for was for development along the**



Columbia River given people's passion for views. This exemplifies why we need to be careful with this.

Maximum lot coverage of buildings and structures is 50%. Plans appear to be under the 50% maximum.

For multifamily dwelling development, the standards of the AR zone defers to the Site Development Review chapter for minimum landscaping requirements; this is detailed further below.

\* \* \*

**Sensitive Lands:** There are sensitive lands as identified in the Development Code. This includes:

- Natural drainageways;
- Wetlands regulated by other agencies such as the US Army Corps of Engineers and/or the Oregon Division of State Lands; and
- Wetlands designated as significant by the City (Chapter 17.40 SHMC).

Prior to this CUP application, a Sensitive Lands Permit was submitted and approved by the City last year (file **Sensitive Lands Permit, SL.2.18**). This addresses potential land preparation work as it pertains to these sensitive lands, but with no specific use proposal.

#### **Natural drainageways.**

Condition 2 of SL.2.18 reads:

**The following shall be required before any grade/fill or other permit, or any work that would impact the site's drainageways:**

A drainage plan certified by a registered professional engineer shall be required that demonstrates that the flow capacity of all drainageways will not be decreased. The modified overland drainage shall provide capture and conveyance of all off-site stormwater that naturally conveys through the site.

Condition 4 of SL.2.18 reads:

This Sensitive Lands Permit alone doesn't allow subsurface drainage methods. This could be considered with subsequent development of the site, but is not guaranteed.

The current proposal includes above ground basins, underground pipes and outflows leading to the wetlands to remain, generally. Given use of the wetlands, final drainage and stormwater plans shall demonstrate compliance with Chapter 17.40 SHMC (e.g., addressing negative impacts such as > water volume, pollutants, and discharge velocity/erosion).

**Wetlands not "significant" to the City.**



Much of the wetland area will be reduced in size and will require authorization from both the US Army Corps of Engineers and/or the Oregon Division of State Lands. Proof of this will be needed before work begins.

**Wetlands “significant” to the City.**

Amongst the wetlands on the site is MC-22 which is significant to the City per its local wetlands inventory and located on the east side of the site. Wetland MC-22 is a Type II significant wetland. As such, it has a 50’ upland protection zone that is protected similar to the wetland itself.

As required by this Chapter, a wetland delineation has been completed via DSL WD #2012-0058R.

Proposed impacts to the protection zone includes a small area east of the “L” shaped building (building #5) on the opposite side of the parking area, a trail/pathway and stormwater outflows. The protection area is not proposed to be decreased by any significant amount.

Other wetland area will also be preserved. Note the Federal wetland setback identified on the site plan and the “extra” wetlands being preserved outside of the MC-22 upland protection zone east of buildings 6, 10 and 11. Preserving this area more than makes up for a minimal loss of MC-22 protection zone.

Wetland and related protection zones to be preserved need protections during construction. Also Wetland/protection zone management plan will be required.

\* \* \*

**Density Computations:** For 138 units, the net developable area needs to be at least 359,000 square feet or 8.24 acres. This is about half of the total size. Density proposed is acceptable.

\* \* \*

**Landscaping/buffering/screening:** Street trees will be required along the Gable Road frontage. This will be a component of the Gable Road frontage improvements.

There is overhead utility lines along the portion of Gable Road that fronts the subject, thus, street trees need to be “small” per this chapter. This requires a 20’ spacing.

Tree location shall also comply with requirements per 17.72.035(2)(d)-(l).

**This chapter requires buffering. This applies in this case as follows:**



- Site abuts GC zoned properties occupied by commercial uses along its SW side. The normal requirement is minimum 10' of buffer plus screening. Plans show this conceptually. Six foot high sight-obscuring fence is proposed.
- Site abuts undeveloped Light Industrial property on its E and NW sides. The normal requirement is minimum 25' of buffer plus screening.

On the East side, the wetlands to be preserved achieve this except near Gable Road. At this location an off-street parking area is proposed. Parking areas near Light Industrial property do not require buffering or screening (at least as it pertains to conflicting land uses).

On the Northwest side, there is at least approximately 90' between the closest building and property line. Parking, landscaping and such is in between. A six foot high sight-obscuring fence is proposed. The site layout meets the purpose of this buffering requirement.

**This chapter requires screening (unrelated to buffering above). This applies in this case as follows:**

Because the parking lot will be greater than three spaces, it is required to be screened. For screening in this case, the City usually requires landscaping along the perimeter that includes a balance of low lying and vertical shrubbery and trees.

Site plan shows this conceptually.

Service facilities and equipment (e.g., HVAC and other mechanical units) visible from a public street, customer or residential parking area, any public facility or residential area are required to be screened whether they are ground, wall or roof mounted. In addition, rooftop facilities and equipment are required to be screened from street and adjacent properties.

Plans provided don't show this level of detail. Screening required in all cases.

Refuse container or collection area are required to be screened (e.g., trash enclosure). Three trash enclosures are proposed and a trash room to be provided in the Senior Housing building. Site plan identifies a 6' high screen as well as a cover and floor drain. This is discussed further below in regards to Chapter 17.92 SHMC and storm and sanitary sewer utilities.

**Interior parking lot landscaping.** When off-street parking lots have more than 20 spaces, landscape islands are required with trees. Rows of parking spaces are not to exceed 7 spaces, generally. The "islands" are required to be no less than 48 square feet in area and no dimension less than six feet. They are required to have a combination of groundcover and shrubs in addition to a tree, such that at least 50% of the island will be covered with living plants. They are also required to be protected from vehicular damage by some form of wheel guard or curb that is permanently fixed to the ground.



General planter detail is provided by sheet D-2. Standard 6" curbs are proposed. All islands measure 7' wide (6' landscape area + 6" curb + 6" curb). Rows of parking spaces do not exceed 7'. Plans reflect necessary details commensurate with this stage of development.

\* \* \*

**Visual Clearance:** Chapter 17.76 SHMC requires proper sight distances at intersections to reduce traffic hazard potential. The required area to maintain clear vision is greater for arterial streets.

Plans identify the vision clearance areas for the proposed accesses off Gable Road. Note that an occasional tree or pole are allowed within the vision clearance area. However, as the Gable Road/US30 intersection is the City's busiest, it will be better to leave trees out of the vision clearance areas, especially since they will be "small" per Chapter 17.72 SHMC with a spacing of 20'.

\* \* \*

**Off-Street Parking/Loading:** Off street parking is required as a new use on vacant property.

**Dimension and type.** There is a mix of standard spaces (min. size 9' x 18'), compact spaces (min. size 8' x 15') and parallel spaces (min. size 8' x 22').

**Visitor Parking.** Multidwelling complexes with more than 10 required spaces are required to have dedicated visitor parking. Visitor parking is also required to be centrally located or evenly distributed throughout the site.

66 of the total 434 off-street parking spaces proposed are for visitors.  $66/434 = 0.1521$  or 15%. Also, visitor parking is spaced throughout.

The visitor parking section also notes that bicycle facilities be centrally located or evenly distributed. Applicant proposes outdoor short term bicycle parking (in addition to bike parking for residents—discussed further below—and those bike racks are distributed throughout.

**Location.** Parking spaces are required to be within 200' of building served. Off-street parking is distributed throughout the site. All but about 6 spaces in the SE corner of the site are within the required distance. Those six spaces are within less than 230 feet. This is a minor issue considering the development footprint left after the wetlands are considered.

**Accessible (disabled person) spaces.** Required to comply with State and Federal Standards. 434 off-street parking spaces are proposed. This requires at least 9 accessible spaces, two of which are required to be van-accessible/wheelchair use only. The plans show 10 accessible spaces.

Also, accessible parking spaces are required to be located on the shortest route to an accessible pedestrian entrance. Though this is a building code issue, it is relevant to site design. Applicant



appears to make an attempt at this, though some space may need to move once final development plans are submitted.

**Bicycle parking.** 1 lockable space is required per dwelling unit. Bicycle spaces are required to be within 50' of primary entrances, under cover when possible, and not located in parking aisles, landscape areas, or pedestrian ways.

The plans identify both short term spaces (uncovered bike racks outside of buildings) and long term bike spaces. The long term spaces are intended for residents. Except for the dwelling units in the Senior Housing building, each unit will have a bike accommodation. A wall mount bike rack is identified in the plans. This will need to be shown on the final plans for the units of all buildings except the Senior Housing building.

The Senior Housing building is shown to have a bike room in its basement and smaller bike rooms on the other floors. Though the plans show 40 bikes in the basement and 10 per floor (total of 70), the applicant noted (via phone) that more bikes can fit. In any case, the final plans will need to show accommodation of at least one bike space amenity per dwelling unit in that building.

The long term bicycle parking will be within buildings and thus covered. To ensure compliance, long term bicycle parking intended for residents shall be covered as a condition of approval.

**Number of off-street parking spaces required.** For multidwelling units the number of off-street parking spaces required is as follows:

- Studio: 1 space per unit
- One bedroom: 1.5 spaces per unit
- Two+ bedrooms: 2 spaces per unit

The breakdown of off-street parking requirement is as follows:

Of the 238 dwelling units proposed, there are the following types (for parking calculations):

- |                       |   |                             |
|-----------------------|---|-----------------------------|
| ○ Studio: 0 units     | → | requires 0 parking spaces   |
| ○ One bed: 72 units   | → | requires 108 parking spaces |
| ○ Two+ bed: 166 units | → | requires 332 parking spaces |

**Total required: 440**

Total off-street parking shown on the plans is 434 spaces. This is 6 spaces or about 1% deficient. Is the Commission ok with this? Basis to be ok with this is noted later in this report (i.e., provisions in the Variance chapter allow for a reduction if certain findings can be made).

Note that the 36 spaces identified just north of building 10 is actually 32 spaces, but this plan error doesn't affect the total count.



Compact spaces may make up to 40% of total spaces. 169 compact spaces are proposed.  $169 / 434 = 0.3894$  or about 39%.

Compact and/or visitor off-street parking spaces shall be signed or labeled as such.

**Aisle width.** A minimum of 24' width is required for two-way vehicular circulation. Plans comply where there is perpendicular off-street parking. Some areas have a 20' width.

20' width is ok, where is no perpendicular parking. 20' is the minimum per fire code. That said, no parking/fire lane identification will be required were appropriate.

**Markings.** All interior drives and access aisles are required to be marked and signed to indicate direction flow. All required off-street parking spaces are required to be clearly marked too. This is reflected on plans, generally.

**Surface area.** All areas used for parking, storage or maneuvering of vehicles (including things towed by vehicles) shall be paved. Plans do not indicate otherwise.

**Wheel stops.** Wheel stops are required along the boundaries of a parking lot, adjacent to interior landscape area, and along pedestrian ways. Most applicable off-street parking spaces show this. Some don't however (e.g., compact spaces in the SW corner). This shall be a condition.

**Drainage.** Drainage plans will be required to prevent ponding, prevent water flow across pedestrian ways and to address pollutants from vehicles (e.g., oil/water separation).

**Lighting.** Pole and bollard lighting is proposed throughout to illuminate the parking areas and pedestrian paths. It is required to be directed to avoid glare from surrounding residences and roads/streets.

Per the [MWA Architects Memorandum dated and received August 14, 2019](#), the applicant proposes to specify light fixtures and a photometric plan as a condition of approval.

\* \* \*

**Access/egress/circulation: Joint access and reciprocal access easements.** Joint access via easement is allowed by the code provided there is satisfactory legal evidence of such (e.g., easements) and the legal means of allowing the shared access is provided to the City.

No shared access is proposed or warranted.

Also note that the Traffic Impact Analysis provided with the application talks about a potential shared access, however, that concept was not included with the CUP application once deemed complete.



**Public street access.** All vehicular access and egress per Chapter 17.84 SHMC is required to directly connect to a public or private street approved by the City for public use. Moreover, vehicular access is required to be within 50' of principle entrances.

The site abuts the following streets:

Street/Road Name	Public or Private	Street Class (TSP)	Jurisdiction	Improved?
Gable Road	Public	Minor Arterial	City of St. Helens	partial (e.g., no sidewalks or other frontage improvements)

The site utilizes this street for access and brings vehicle access within the statutory distance of the primary entrances.

#### **Vehicular access spacing, amount, etc.**

Two access points are proposed directly from Gable Road.

There are several vehicular access considerations:

1. The access spacing standards along Minor Arterial roads is 200' measured from center of other driveways or streets.

The St. Helens Transportation Systems Plan calls for a northerly extension of McNulty Way. The location of McNulty Way and the wetlands do not make this suitable for the subject property. However, the McNulty Way extension is possible for the adjacent property on the east side. Plans show that the easternmost proposed driveway is 200' from the northerly extension of McNulty Way.

On the other side, the other direct Gable Road access is >200' from the existing driveway serving the Wilson Oil property at 2290 Gable Road.

And the distance between the two proposed direct Gable Road accesses is >200 feet.

2. Pursuant to SHMC 17.84.040(7):

(a) New residential land divisions fronting onto an arterial street shall be required to provide secondary (local or collector) streets for access to individual lots. When secondary streets cannot be constructed due to topographic or other physical constraints, access may be provided by consolidating driveways for clusters of two or more lots (e.g., includes flag lots and mid-block lanes).

(b) Nonresidential projects proposed on arterials shall include a frontage or service road and shall take access from the frontage or service road rather than the arterial. Frontage or service road design shall conform to applicable jurisdictional design standards. This access requirement may be met through the use of interconnecting parking lots that abut the arterial provided the necessary easements and agreements are obtained.



The idea is to minimize impacts along arterial streets by minimizing accesses. Gable is the only option for access. The site is separated from US30 by a railroad. US30 is an arterial street anyways.

3. Pursuant to SHMC 17.84.040(8):

The number of street access points for multiple dwelling unit residential, commercial, industrial, and public/institutional developments shall be minimized to protect the function, safety and operation of the street(s) and sidewalk(s) for all users. Shared access may be required, in conformance with subsection (9) of this section, in order to maintain the required access spacing, and minimize the number of access points.

4. Design cannot be such that maneuvering or backing movements are required in Gable Road.
5. Based on the number of dwelling units proposed, a minimum of three access points are required.

However, note that on page 16 of the Traffic Impact Analysis submitted with the application it notes that two driveways can provide access to the site while maintaining acceptable traffic operations.

6. Pursuant to SHMC 17.04.040(2):

Where this code imposes greater restrictions than those imposed or required by other rules or regulations or code provisions, the provisions of this code shall control.

In sum, we want to minimize impact to Gable Road as much as possible. The two direct accesses proposed meet the access spacing standards and will function as attested by Kittelson and Associates who conducted the traffic impact analysis. The two accesses proposed meets the intent of the code.

Moreover, it is still possible to connect to the northerly extension of McNulty Way, which is planned to occur per the Transportation Systems Plan.

**Pedestrian access (interior walkways).**

Within all attached housing and multifamily developments, each residential dwelling shall be connected by walkway to the vehicular parking area, and common open space and recreation facilities. Walkways are also required from ground floor entrances or landings of stairs, etc. to streets that provide access.

There is a network of walkways throughout the site. Based on preliminary floorplans, all ground floor entrances appear accommodated. However, this will have to be further evaluated with final development plans.

Where a site for proposed commercial, institutional, or multifamily development is located within at least one-quarter mile of an existing or planned transit stop, the proposed pedestrian



circulation system must include a safe and direct pedestrian walkway from building entrances to the transit stop or to a public right-of-way that provides access to the transit stop.

Per the 2009 Columbia County Community-Wide Transit Plan there is a transit stop within a quarter mile (around Safeway / Rite Aid). This project will include sidewalk and other frontage improvement construction along Gable Road that will connect to existing sidewalks that lead to the Gable Road / US30 intersection along the way to the transit stop. There is also a stop across Gable Road within the Wal-Mart property (per the CC Rider website), which the street frontage improvements will help provide access to too.

In parking lots one acre or larger, pedestrian walkways shall connect from buildings to sidewalks in the adjacent rights-of-way, and shall be provided at least every 150 feet between rows of parking.

Though larger than an acre, the parking area surrounds the buildings and is interspersed throughout. This provisions is meant for large contiguous off-street parking lots.

Wherever required walkways cross vehicle access driveways or parking lots, such crossings shall be designed and located for pedestrian safety. Required walkways shall be physically separated from motor vehicle traffic and parking by either a minimum six-inch vertical separation (curbed) or a minimum three-foot horizontal separation, except that pedestrian crossings of traffic aisles are permitted for distances no greater than 36 feet if appropriate landscaping, pavement markings, or contrasting pavement materials are used. Walkways shall be a minimum of four feet in width, exclusive of vehicle overhangs and obstructions such as mailboxes, benches, bicycle racks, and sign posts, and shall be in compliance with ADA standards.

Several formal crossings are proposed. Pavement markings are proposed. No crossing is greater than 36 feet.

Required walkways shall be paved with hard-surfaced materials such as concrete, asphalt, stone, brick, etc. Walkways shall be required to be lighted and/or signed as needed for safety purposes. Soft-surfaced public use pathways may be provided only if such pathways are provided in addition to required pathways.

Paved walkways are proposed throughout. Except a portion just NE of the Senior Housing building, where a paved walk abruptly ends.

Lighting by walkways them is proposed throughout.

**Access requirements based on type and intensity of use.** With 238 dwelling units, at least three accesses to a public street area required. The “two v. three” access issue is discussed above. They are required to be at least 24’ in width and include curbs and minimum 5’ sidewalk. The two proposed new accesses meet this.

\* \* \*



**Signs:** No signs are proposed at this time. New signs will require permits per Chapter 17.88 SHMC.

\* \* \*

**Solid Waste/Recyclables:** Chapter 17.92 SHMC includes provisions for functional and adequate space for on-site storage and efficient collection of mixed solid waste and recyclables subject to pick up and removal by haulers.

Multidwelling unit uses require 50 s.f. of space for this plus an additional 5 s.f. for each unit above 10. At 238 units, there are 228 above 10.

Thus  $50 \text{ s.f. (for 1}^{\text{st}} 10 \text{ units)} + (228 \times 5) = 1,190 \text{ s.f. of minimum area required.}$

Three trash enclosures are proposed outside of buildings. The sum of these exceeds the minimum area requirement. In addition, a trash area is proposed in the basement level of the Senior Housing building.

Multiple locations and a mix of indoor and outdoor is acceptable per the code. The outdoor trash enclosures are in visible areas and reasonably distributed.

Enclosures will have a 6' screen with 10' gates, as narrated on the site plan. Also, as storage areas will be covered a minimum of 10 feet horizontal clearance and eight feet of vertical clearance is required. Detail plans will be required with development permits.

As being proposed to be covered with a floor drain, the drain will need to connect to sanitary sewer and the enclosure shall be design to prevent storm water infiltration into the sanitary sewer drain. Storm and sanitary sewer are required to be separate.

Note that trash enclosures may be in side or rear yards (setback); this is a factor to consider for crime prevention (discussed below).

\* \* \*

### **Site Development Review:**

**Per SHMC 17.96.180(2)—Relationship to the Natural and Physical Environment:**

- (a) Buildings shall be:
  - (i) Located to preserve existing trees, topography, and natural drainage in accordance with other sections of this code;
  - (ii) Located in areas not subject to ground slumping or sliding;
  - (iii) Located to provide adequate distance between adjoining buildings for adequate light, air circulation, and fire fighting; and
  - (iv) Oriented with consideration for sun and wind; and
- (b) Trees having a six-inch DBH (as defined by Chapter 17.132 SHMC) or greater shall be preserved or replaced by new plantings of equal character;



The main issue for this is wetlands. Wetlands needed to be protected per City, State and Federal law are not proposed to be removed.

**Per SHMC 17.96.180(3)—Exterior Elevations:**

Along the vertical face of single-dwelling units – attached and multiple-dwelling unit structures, offsets shall occur at a minimum of every 30 feet by providing any two of the following:

- (a) Recesses (decks, patios, entrances, floor area, etc.) of a minimum depth of eight feet;
- (b) Extensions (decks, patios, entrances, floor area, etc.) of a minimum depth of eight feet, and maximum length of an overhang shall be 25 feet; and
- (c) Offsets or breaks in roof elevations of three or more feet in height;

Reviewing the plans by building type per each standard **(a)**, **(b)** and **(c)**:

**Townhouse building** (SW quadrant of site)

Four of these buildings are proposed and are rectangular in dimension. They are two stories in height, with individual units encompassing two stories.

On the ground level they have multiple 11' recesses **(a)** with 5' extensions and multiple 14' extensions **(b)** on the opposing side.

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**Family Housing building** (NW quadrant of site)

Five of these buildings are proposed and are rectangular in dimension. They vary in size with the larger ones having offsets.

Along the long sides, they include recesses 13' in depth **(a)**. They also include breaks in roof elevations at at least 30 foot intervals greater than 3' in height **(c)**.

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**Senior Housing building** (SE quadrant of site)

This is a single "L" shaped building. The focus is on the East and South facing long side elevations as the other long side elevations are not visible from Gable Road and will be screened by the preserved forested wetlands.

There are 5' enclosed (building footprint) extensions at at least 30 foot intervals. These are topped by 3' roof overhangs, bringing the total to 8 feet. This achieves **(b)**. Because the overhangs will be about 29' high, they will also provide the three foot break in roof elevation as seen from below. This achieves **(c)**.

**Per SHMC 17.96.180(4)—Buffering, Screening, and Compatibility between Adjoining Uses:**

(a) Buffering shall be provided between different types of land uses (for example, between single-dwelling units and multiple-dwelling units residential, and residential and commercial), and the following factors shall be considered in determining the adequacy of the type and extent of the buffer:

- (i) The purpose of the buffer, for example to decrease noise levels, absorb air pollution, filter dust, or to provide a visual barrier;
- (ii) The size of the buffer required to achieve the purpose in terms of width and height;



- (iii) The direction(s) from which buffering is needed;
- (iv) The required density of the buffering; and
- (v) Whether the viewer is stationary or mobile;

Buffering beyond that described above is not warranted. Forested wetlands to be kept provide a significant buffer from adjacent industrial lands.

(b) On-site screening from view from adjoining properties of such things as service areas, storage areas, parking lots, and mechanical devices on rooftops (e.g., air cooling and heating systems) shall be provided and the following factors will be considered in determining the adequacy of the type and extent of the screening:

- (i) What needs to be screened;
- (ii) The direction from which it is needed;
- (iii) How dense the screen needs to be;
- (iv) Whether the viewer is stationary or mobile; and
- (v) Whether the screening needs to be year-round;

Screening is addressed elsewhere for most screening needs.

**Per SHMC 17.96.180(5)—Privacy and Noise:**

(a) Structures which include residential dwelling units shall provide private outdoor areas for each **ground floor unit** which is screened from view by adjoining units as provided in subsection (6)(a) of this section;

(b) The buildings shall be oriented in a manner which protects private spaces on adjoining properties from view and noise;

(c) Residential buildings should be located on the portion of the site having the lowest noise levels; and

(d) On-site uses which create noise, lights, or glare shall be buffered from adjoining residential uses (see subsection (4) of this section);

**Townhouse building** (SW quadrant of site)

These units have patio areas that exceed the minimum size and are not accessways for other units. Two units in each building have patios facing each other. A dividing wall is proposed for privacy. **Is 5' high enough?**

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**Family Housing building** (NW quadrant of site)

The private decks for the 2<sup>nd</sup> and 3<sup>rd</sup> floors are not the same for the ground floor as that area acts as access to the building. The plans take some effort to provide privacy for the main door to access the ground floor units. So this questionable.

**However, the applicant proposes to utilize SHMC 17.108.080(3)(c) for this. See below.**

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**Senior Housing building** (SE quadrant of site)



Ground floor units lack this. **However, the applicant proposes to utilize SHMC 17.108.080(3)(c) for this.** See below.

**Per SHMC 17.96.180(6)—Private Outdoor Area – Residential Use:**

(a) Private open space such as a patio or balcony shall be provided and shall be designed for the exclusive use of individual units and shall be at least 48 square feet in size with a minimum width dimension of four feet, and:

(i) Balconies used for entrances or exits shall not be considered as open space except where such exits or entrances are for the sole use of the unit; and

(ii) Required open space may include roofed or enclosed structures such as a recreation center or covered picnic area;

(b) Wherever possible, private outdoor open spaces should be oriented toward the sun; and

(c) Private outdoor spaces shall be screened or designed to provide privacy for the users of the space;

**Townhouse building** (SW quadrant of site)

These units include both the first and second story. Thus (5) above drives the private outdoor area for these building types.

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**Family Housing building** (NW quadrant of site)

These buildings include three stories. The 2<sup>nd</sup> and 3<sup>rd</sup> floors show adjacent private decks that exceed the minimum size with walls in between.

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**Senior Housing building** (SE quadrant of site)

This buildings includes three stories and a basement level. No private outdoor area is provided for any level. **However, the applicant proposes to utilize SHMC 17.108.080(3)(c) for this.** See below

**Per SHMC 17.96.180(7)—Shared Outdoor Recreation Areas – Residential Use:**

(a) In addition to the requirements of subsections (5) and (6) of this section, usable outdoor recreation space shall be provided in residential developments for the shared or common use of all the residents in the following amounts:

(i) Studio up to and including two-bedroom units, 200 square feet per unit; and

(ii) Three- or more bedroom units, 300 square feet per unit;

(b) The required recreation space may be provided as follows:

(i) It may be all outdoor space; or

(ii) It may be part outdoor space and part indoor space; for example, an outdoor tennis court and indoor recreation room;

(iii) It may be all public or common space; or

(iv) It may be part common space and part private; for example, it could be an outdoor tennis court, indoor recreation room and balconies on each unit; and

(v) Where balconies are added to units, the balconies shall not be less than 48 square feet;

(c) Shared outdoor recreation space shall be readily observable for reasons of crime prevention and safety;



This is for *shared outdoor usable recreation space*. Of the total 238 dwelling units proposed, 201 are 1- or 2-bedroom units and 37 are 3-bedroom units.

Thus, the amount of *shared outdoor usable recreation space* is:

$$(201 \text{ units} \times 200 \text{ s.f.}) + (38 \text{ units} \times 300 \text{ s.f.}) = 53,400 \text{ square feet (or about 1.23 acres)}$$

A 4,500 square foot commons building is proposed. And there is approximately 2.5 acres of shared outdoor space per sheet L1.02.

**Per SHMC 17.96.180(8)—Demarcation of Public, Semipublic, and Private Spaces – Crime Prevention:**

(a) The structures and site improvements shall be designed so that public areas such as streets or public gathering places, semipublic areas and private outdoor areas are clearly defined in order to establish persons having a right to be in the space, in order to provide for crime prevention and to establish maintenance responsibility; and

(b) These areas may be defined by:

- (i) A deck, patio, low wall, hedge, or draping vine;
- (ii) A trellis or arbor;
- (iii) A change in level;
- (iv) A change in the texture of the path material;
- (v) Sign; or
- (vi) Landscaping;

Boundaries should be clear.

**Per SHMC 17.96.180(9)—Crime Prevention and Safety:**

(a) Windows shall be located so that areas vulnerable to crime can be surveyed by the occupants;

(b) Interior laundry and service areas shall be located in a way that they can be observed by others;

(c) Mail boxes shall be located in lighted areas having vehicular or pedestrian traffic;

(d) The exterior lighting levels shall be selected and the angles shall be oriented towards areas vulnerable to crime; and

(e) Light fixtures shall be provided in areas having heavy pedestrian or vehicular traffic and in potentially dangerous areas such as parking lots, stairs, ramps, and abrupt grade changes:

(i) Fixtures shall be placed at a height so that light patterns overlap at a height of seven feet, which is sufficient to illuminate a person;

(a) All buildings appear to have windows on all sides to keep “eyes on the street and area” except for the Commons Building, where there is insufficient information.

(b) Need more evolved floor plans to determine this. The Senior Housing building is shown to have a shared laundry area, at least. It also has shared service areas such as the shared bike storage. This shall be a condition of approval to ensure compliance with final plans.

(c) Mailboxes location appears to be in such an area, but location needs final approval by Postmaster.

(d) & (e) Lighting is proposed throughout. Final plans will need to include light pattern details.

**Per SHMC 17.96.180(10)—Access and Circulation:**



- (a) The number of allowed access points for a development shall be as provided in SHMC 17.84.070;
  - (b) All circulation patterns within a development shall be designed to accommodate emergency vehicles; and
  - (c) Provisions shall be made for pedestrianways and bicycleways if such facilities are shown on an adopted plan;
- (a) This is addressed above.
  - (b) No vehicular passageway is less than 20' wide, which is the minimum for Fire Marshall standards. "No parking" designations such to City and Fire Marshall approval will be required where necessary to ensure emergency vehicular access. For example, where passageways are only 20' wide physically.
  - (c) This is addressed above.

**Per SHMC 17.96.180(11)—Distance between Multiple-Family Residential Structure and Other:**

- (a) To provide privacy, light, air, and access to the multiple and attached residential dwellings within a development, the following separations shall apply:
  - (i) Buildings with windowed walls facing buildings with windowed walls shall have a 25-foot separation;
  - (ii) Buildings with windowed walls facing buildings with a blank wall shall have a 15-foot separation;
  - (iii) Buildings with opposing blank walls shall have a 10-foot separation;
  - (iv) Building separation shall also apply to buildings having projections such as balconies, bay windows, and room projections; and
  - (v) Buildings with courtyards shall maintain separation of opposing walls as listed in subsections (11)(a)(i), (ii) and (iii) of this section for walls in separate buildings;

There is at least 25' separation between principle buildings.

- (b) Where buildings exceed a horizontal dimension of 60 feet or exceed 30 feet in height, the minimum wall separation shall be one foot for each 15 feet of building length over 50 feet and two feet for each 10 feet of building height over 30 feet;

Generally, this is met.

- (c) Driveways, parking lots, and common or public walkways shall maintain the following separation for dwelling units within eight feet of the ground level:
  - (i) Driveways and parking lots shall be separated from windowed walls by at least eight feet; walkways running parallel to the face of the structures shall be separated by at least five feet; and
  - (ii) Driveways and parking lots shall be separated from living room windows by at least 10 feet; walkways running parallel to the face of the structure shall be separated by at least seven feet;

There is 10 foot separation, with the following exceptions:

- Building 7: walkways as close as 5' to bedroom windows, which is ok
- Buildings 1 and 3: walkways as close as 7' to living room walls, which is ok

**Per SHMC 17.96.180(12)—Parking:**



All parking and loading areas shall be designed in accordance with the requirements set forth in SHMC 17.80.050 and 17.80.090; Chapter 17.76 SHMC, Visual Clearance Areas; and Chapter 17.84 SHMC, Access, Egress, and Circulation;

Parking addressed herein.

**Per SHMC 17.96.180(13)—Landscaping:**

(a) All landscaping shall be designed in accordance with the requirements set forth in Chapter 17.72 SHMC; and

(b) For residential use, in addition to the open space and recreation area requirements of subsections (6) and (7) of this section, a minimum of 15 percent of the gross area including parking, loading and service areas shall be landscaped;

The parking lot landscaping and wetlands to be preserved addresses the 15% requirement.

**Per SHMC 17.96.180(14)—Drainage:**

All drainage plans shall be designed in accordance with the criteria in the most current adopted St. Helens master drainage plan;

Drainage plan will be required. Also, since this include public storm drain lines, civil engineering of the infrastructure is necessary.

**Per SHMC 17.96.180(15)—Provision for the Handicapped:**

All facilities for the handicapped shall be designed in accordance with the requirements pursuant to applicable federal, state and local law;

This will be addressed more thoroughly with building permit and engineering/construction plan review.

**Per SHMC 17.96.180(16)—Signs:**

All sign placement and construction shall be designed in accordance with requirements set forth in Chapter 17.88 SHMC;

Signs shall require permits as applicable.

**Per SHMC 17.96.180(17):**

All of the provisions and regulations of the underlying zone shall apply unless modified by other sections of this code (e.g., the planned development, Chapter 17.148 SHMC; or a variance granted under Chapter 17.108 SHMC; etc.).

There is no planned development or Variance proposed.

However, given some deficiency in private outdoor area requirements as noted above, the applicant proposes to utilize SHMC 17.108.080(3)(c) in the Variance Chapter:

(3) The director may grant an exception or deduction to the private outdoor area and shared outdoor recreation areas requirements, provided the application is for a use designed for a specific purpose which is intended to be permanent in nature (for example, senior citizen housing) and which



can demonstrate a reduced demand for a private outdoor recreational area based on any one or more of the following findings:

(c) The required square footage of either the private outdoor area or the shared outdoor recreation area may be reduced if together the two areas equal or exceed the combined standard for both.

The shared outdoor area (*shared outdoor usable recreation space*) exceeds the minimum considerably. The minimum required and proposed is 1.23 ac. and 2.5 ac., respectively. This makes up for that. **But the Commission has to agree with this because the exception “may” be granted, not “shall” be granted.**

We can also look at SHMC 17.108.080(2)(a) and (c) in the Variance chapter to address the approximate 1% shortfall of total required off-street parking. A deduction of off-street parking minimum number of spaces required may be allowed based on:

(a) The application is for a use designed for a specific purpose which is intended to be permanent in nature (for example, senior citizen housing) and which has a demonstrated low demand for off-street parking;

(c) There is community interest in the preservation of particular natural feature(s) on the site, public transportation is available to the site, and reducing the standards will not adversely affect adjoining uses; therefore, the public interest is not adversely affected by the granting of the exception.

As noted above, total parking required is 440 spaces and the applicant proposes 434; thus, 6 spaces deficient. Staff feels that (c) can be applied given the site’s wetlands, which take up considerable area. **But the Commission has to agree with this because the deduction “may” be granted, not “shall” be granted.**

\* \* \*

### **Conditional Use:**

#### **SHMC 17.100.040(1):**

(1) The planning commission shall approve, approve with conditions, or deny an application for a conditional use or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:

- (a) The site size and dimensions provide adequate area for the needs of the proposed use;
- (b) The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features;
- (c) All required public facilities have adequate capacity to serve the proposal;
- (d) The applicable requirements of the zoning district are met except as modified by this chapter;
- (e) The supplementary requirements set forth in Chapter 17.88 SHMC, Signs; and Chapter 17.96 SHMC, Site Development Review, if applicable, are met; and
- (f) The use will comply with the applicable policies of the comprehensive plan.

Generally, this is addressed elsewhere in this report, except compliance with the comprehensive plan. One comprehensive plan policy of note is in the Housing section 19.08.05(3)(e)(vi):

(e) Permit multifamily developments which conform to the following general conditions and criteria:



(vi) They should not be located where undue noise or other factors will adversely affect residential living.

**For the Commission's consideration give proximity to US30, Gable Road and the railroad.**

**SHMC 17.100.040(3):**

(3) The planning commission may impose conditions on its approval of a conditional use, which it finds are necessary to ensure the use is compatible with other use in the vicinity. These conditions may include, but are not limited to, the following:

- (a) Limiting the hours, days, place, and manner of operation;
- (b) Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust;
- (c) Requiring additional setback areas, lot area, or lot depth or width;
- (d) Limiting the building height, size or lot coverage, or location on the site;
- (e) Designating the size, number, location, and design of vehicle access points;
- (f) Requiring street right-of-way to be dedicated and the street to be improved;
- (g) Requiring landscaping, screening, drainage and surfacing of parking and loading areas;
- (h) Limiting the number, size, location, height, and lighting of signs;
- (i) Limiting or setting standards for the location and intensity of outdoor lighting;
- (j) Requiring berming, screening or landscaping and the establishment of standards for their installation and maintenance;
- (k) Requiring and designating the size, height, location, and materials for fences; and
- (l) Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.

**Discussion:** These are items the Commission may consider for this proposal. Things for the Commission to consider include but are not limited to:

1. Needs new address. Address for former transfer station doesn't match the odd and evens pattern. *This is discussed herein.*
2. **Building height for the Family Housing buildings as a condition of approval.** *This is discussed herein*
3. No trees within vision clearance area per Chapter 17.76 SHMC. *This is discussed herein and is the applicants intent too.*
4. **Anything pertaining to the Comprehensive Plan policy regarding undue noise or other factors that could adversely affect residential living.** *This is discussed herein.*
5. There are two properties being developed as one. Properties need to be combined to prevent future disparate ownership.
6. An essential aspect of crime prevention is creating a sense of being watched. In most areas there are windows facing outward covering the site. However, there is a potential weak area in this regard: east of the commons area building.

Floorplans of that building shows electric/mechanical room, a storage room and bike shop storage on its east side. Elevation of that building's east side were not provided, but this elevation is less likely to have windows due to the nature of the rooms and uses on that side. Windows or faux windows shall be required on that elevation of such design to meet the intent of crime prevention. For example, small utility windows not easily viewed from a distance would not meet this intent.

\* \* \*



**Accessory Structures:** The proposed principle use is a 238 unit multidwelling complex. The only accessory structures proposed are the trash enclosures.

**Maximum size allowed:** Maximum *gross floor area* (GFA) allowed for an accessory structure in a residential zone (multidwellings require AR zone compliance in the GC zone) depends on property size. The maximum GFA is 600 unless the property (lot or parcel) it is proposed on is at least 2½ acres in size. The property is much more than 2½ acres in size, which increases the maximum size allowed to 1,000 GFA. Trash enclosures are all less than 1,000 GFA.

**Lot coverage:** Accessory structures cannot exceed the coverage allowed combined with the principle structures/buildings. This looks ok.

**Yard requirements:** the yard (setback) requirements of the zoning district are required to be met. The accessory structures meet the proper yard requirements. As the trash enclosures are covered, they are considered structures and thus need to meet the same yard requirements as the principle buildings.

**Architectural requirements:** The style and exterior materials are required to be similar as the principle building and the roof pitch of the accessory building is required to be the same as the principle building if the accessory building exceeds 15 feet in height. No renderings or elevations provided. Shall comply with this provision.

**Tree Removal/Preservation:** Chapter 17.132 SHMC addresses the preservation of trees with a diameter at breast height (DBH) >12 inches. Protection is preferred over removal per this Chapter and Site Development Review Chapter 17.96 SHMC.

The majority of the trees on this site are within wetland areas proposed to be preserved. This includes the upland protection zone for the wetlands that are significant to the City.

The applicant's existing conditions plan identifies about five trees outside of the wetlands and wetland protection zone to be preserved. Two of those are proposed to be preserved.

Given street tree, parking lot island trees and other landscaping trees, the trees to be removed will be replaced at a high ratio.

A protection program by a qualified professional defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building Permit plan set to ensure contractors and others follow the tree protection plan during site development.

Note that the Geotechnical Design Investigation dated May 23, 2019 submitted with the application identified challenging conditions to preserve trees were development is proposed to occur.



Site plan contradicts other plans only showing one tree preserved. Plans shall not be contradictory.

\* \* \*

**Street/Right-of-Way Standards:** Gable Road is underdeveloped and will be required to be improved per the City's minor arterial classification as identified in the City's 2011 Transportation Systems Plan (Ord. No. 3150 and 3181).

This was updated by the City's Riverfront Connector Plan (Ord. 3241). This ordinance was approved and adopted by the Council on August 7, 2019. Per Section 7 of that Ordinance, the Plan and related code amendments take effect 30 days after adoption (i.e., September 6, 2019).

This application was deemed complete on August 26, 2019 with receipt of payment. The City cannot impose development standards that were not in effect when the application was deemed complete. However, we can recommend the new standards.

In regards to street lighting, there are cobra head street lights attached to utility poles fronting the subject property. At a minimum, need to make sure new Gable Road driveways will be adequately illuminated for safety.

\* \* \*

**Utility Standards:**

**Water:** There is a water main within the Gable Road right-of-way. The main is proposed to be extended into the site partially within an easement along with a proposed sanitary sewer line.

It needs to be clear where the public water main ends and the private line starts. Demarcation of that needs to be a part of this development, in a form approved by the City. Also, water meter and backflow device will be required at the public/private junction.

**Sanitary Sewer:** There is a sanitary sewer main within the Gable Road right-of-way. The main is proposed to be extended into the site partially within an easement along with a proposed water line. As with water line, demarcation of public and private on site will be necessary.

**Storm Sewer:** There is no formal drainage on Gable Road, so it is best to keep the storm water on site.

A couple storm lines within easements are proposed to convey off-site water from the west to the wetland areas to be preserved. They are proposed to be within easements and intended to be public. City does not object to this concept. Demarcation of public and private systems will be required as with water and sanitary sewer.

Otherwise, a mix of above ground basins and piping is intended to convey on site storm water to the wetlands to be preserved. This infrastructure will be private.



Storm is proposed to be discharged to the wetlands. The stormwater plan will need to address preventing negative impacts to the wetlands such as water volume, pollutants and discharge velocity/erosion.

**Other:** new utilities are required to be underground. Overhead utilities along Gable Road may remain, but new poles shall not be allowed.

\* \* \*

**Trails/bikeways:** There is no identified trail in the current Parks and Trails Master Plan. Bike lanes are a component of the City's standards for Gable Road.

There are no identified trails in the City's Parks and Trails Master Plan (2015) through the subject property.

\* \* \*

**Traffic Impact Analysis (TIA):** The proposal is large enough that a TIA is required. Both the City and ODOT have reviewed the analysis, though, ODOT's review is limited as no direct access is proposed onto a State facility (i.e., US30).

The study indicates that the proposal can be constructed while maintaining acceptable traffic operations and safety at the intersections studied. There are some issues however, as discussed further below.

Acceptable function of City controlled intersections is based on level of service (LOS), where intersections are graded "A" – "F." The analysis provided indicates that city controlled intersections will have a LOS of "A" or "B" and within an acceptable range. All City controlled intersections will operate within the applicable LOS per the TIA.

Intersections under ODOT (State) jurisdiction within the TIA study area have a mobility target measured by a volume/capacity ratio (V/C) of 0.85. There are issues.

Key items for discussion in this report include the Gable Road/US30 intersection, the Millard Road/US30 intersection, and the segment of Gable Road between US30 and McNulty Way.

**Gable Road/US 30 intersection.** This intersection currently exceeds its applicable mobility target under *existing* traffic conditions during the *weekday PM peak hour*. With completion of this proposal, the intersection is expected to exceed the mobility target during the *weekday AM and PM peak hours*. So the AM peak becomes an issue and the PM peak worsens.

Upon build out, the AM peak is anticipated to have a V/C of 0.87 and the PM peak with a V/C of >1.0. >1 means above capacity; the volume will exceed capacity regardless of the standard that applies (i.e., v/c of 0.85).



In the City's Transportation Systems Plan, there is a long term improvement (Project L01 in Table 7-7 in the City's 2011 TSP) to install a westbound right-turn lane. This project has an estimated cost of \$485,000 (c. 2011 dollars). Note that cost estimates are higher in the Riverfront Connector plan (>\$2,000,000) but that plan is not effective at the time of this CUP's completeness.

As done with other recent projects that have affected the functionality of this intersection (e.g., files SDR.1.18 and CUP.2.18), the applicant shall be required to pay a proportionate share of the cost of the westbound right-turn lane at this intersection. The project cost shall be based on the estimated cost in the TSP with inflation to current. Note that 29.5% of this cost is SDC creditable.

**Millard Road/US 30 intersection.** This intersection currently exceeds its applicable mobility target under *existing* traffic conditions during the *weekday AM and PM peak hours*. This proposal will add to that.

ODOT is planning to install a traffic signal along with other intersection improvements. According to ODOT's website (<https://www.oregon.gov/odot/projects/pages/default.aspx>), construction of this \$7.5 million project is currently forecast for 2021.

The ODOT project is anticipated to make up for any functional issues this proposal result at this intersection.

**Segment of Gable Road between US 30 and McNulty Way.** Staff was concerned about this segment and wanted analysis to show function and if any additional traffic control measures. The TIA identifies existing and proposed driveways along this segment functioning adequately upon build out of the proposal.

However, the TIA notes the center turn lane and that once properties to the east develop, the existing center turn lane can be extended eastward (center two-way left turn lane), which would significantly improve operations at the site driveways. Note that the current TSP minor arterial cross section doesn't include a center lane, whereas the cross section for this segment of Gable Road in the Riverfront Connector Plan does. For traffic functionality, the street design shall include a center turn lane/median.

\* \* \*

**Other Considerations:** Property identified as 4N1W-8AA-301 has a past address of 2285 Gable Road. Generally, addresses on Gable Road end with an even number on the north side. 2285 is out of place.

Past files identify 4N1W-8AA-200 as 2222 Gable Road, though, there is no formal record. This doesn't appear to be an official address.

New address will need to be assigned when development permits are issued.



Mailbox facility requires Post Master approval; this is necessary for final development plans. Staff spoke to the Post Master about the proposal on August 29, 2019 and the location seemed ok but the ultimate design needs to: 1) limit drive time and distance within the facility (e.g., current design would require mail carrier to drive around the Family Housing buildings), 2) prevent backing movements (a dedicated parking space may allow an exception to this) and 3) include package locker considerations.

\* \* \* \* \*

### CONCLUSION & RECOMMENDATION

**Based on the facts and findings herein, if the Planning Commission approves this Conditional Use Permit, staff recommends the following conditions:**

1. This **Conditional Use Permit** approval is valid for a limited time (to establish the use) pursuant to SHMC 17.100.030. This Conditional Use Permit approval is valid for 1.5 years. A 1-year extension is possible but requires an application and fee. If the approval is not vested within the initial 1.5 year period or an extension (if approved), this is no longer valid and a new application would be required if the proposal is still desired. See SHMC 17.100.030.
2. The following shall be required prior to any development or building permit issuance:
  - a. Final plans as submitted with any development or building permit(s) shall comply with the plans submitted with this Conditional Use Permit with the following additions and/or corrections:
    - A. Landscaping vegetation between off-street parking area drive aisles and buildings shall not be greater than 10 feet at maturity.
    - B. The off-street parking lot just west of buildings 1 and 4 shall have an emergency access to Gable Road with removable bollards.
    - C. "Small" street trees as defined by Chapter 17.72 SHMC along Gable Road.
    - D. Landscaping specifications to meet the buffering and associated screening requirements per the Development Code and as described herein.
    - E. Landscaping specifications to address "screening" of the parking lot as seen from Gable Road and neighboring properties to include a balance of low lying and vertical shrubbery and trees.
    - F. As per condition 5.
    - G. As per condition 6 in regards to utility connection, as applicable.
    - H. Landscape specifications of all other proposed landscaping, including but not limited to interior parking lot "islands" which are required to have a combination of



groundcover and shrubs in addition to a tree, such that at least 50% of the island will be covered with living plants at maturity.

- I. No street trees shall be allowed within vision clearance per Chapter 17.76 SHMC areas for general use driveways. This doesn't apply to emergency use only driveway.
- J. A bike space amenity shall be shown on the plans with 1 per unit within the unit as proposed for the dwelling units. The exception is for the Senior Housing building units, which shall show 1 bike space amenity per unit in common areas within the building as proposed.
- K. In addition to normal parking space markings, compact and/or visitor off-street parking spaces shall be signed or labeled as such. Plans shall indicate method.
- L. "No parking/fire lane" identification shall be required where appropriate. For example, where passageways are only 20 feet wide.
- M. Wheel stops are required along the boundaries of a parking lot, adjacent to interior landscape area, and along pedestrian ways.
- N. Primary walkways are required to be paved. Shall be properly addressed. For example, the paved walk abruptly ends just NE of the Senior Housing building.
- O. Specifications of trash enclosures as proposed. Note condition 6. If covered, and thus considered accessory structures, they must be the same yard (setback) requirements of the principle buildings. If not covered, trash enclosures may be within the yard; in this case they shall back up to the perimeter fence (where possible), to eliminate a hiding place for crime prevention purposes.  
  
Further, if covered and considered accessory structures, elevations shall be provided demonstrating that the style and exterior materials will be similar to the principle buildings. Also, if covered a minimum of 8' vertical clearance is required.
- P. The shared laundry and service areas in the Senior Housing building shall be designed such that they can be readily observed by others.
- Q. Joint mailbox facilities are required to be shown on plans and be approved by the City and US Post Office (Postmaster) prior to final plan approval. An accessible route is required. Mailbox facility(ies) shall be located in lighted areas having vehicular or pedestrian traffic. Note comments from a discussion with the Postmaster herein.
- R. No street trees within the vision clearance area per Chapter 17.76 SHMC. This chapter normally allows occasional utility pole or tree. This condition doesn't apply to emergency access (occasional trees may be in vision clearance area of that).



- S. The east façade of the Commons Building shall have windows or faux windows to give a sense of being watched for crime prevention purposes. The design shall meet the intent of crime prevention. For example, small utility windows not easily viewed from a distance would not meet this intent.
  - T. A protection program by a qualified professional defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building Permit plan set to ensure contractors and others follow the tree protection plan during site development.
  - U. Require new building elevations with an overall reduced height for the Family Housing buildings or a Variance to building height, or as a condition of this CUP? See discussion on pages 4-5. *Location of this condition can change depending on decision.*
  - V. If the Commission doesn't find that an approximate 1% (6 spaces) deduction as discussed on pages 9 and 21 is justified for this proposal, revised plans will be necessary.
  - W. Does the Commission find that the deficiencies in *private outdoor area* requirements as discussed on pages 16-17 and 20-21 is justified by the extra *shared outdoor usable recreation space*. If not, revised plans will be required. Or because this would be a substantial change, the public hearing may need to be continued to allow time for the applicant to address the issues.
- At the very least the proposed dividing wall proposed for the private outdoor areas of the townhouse buildings that face each other should be at least 6 feet in height (plan shows 5 feet).*
- X. Does the Commission want to require anything given the Comprehensive Plan policy that multifamily developments should not be located where undue noise or other factors will adversely affect residential living? For example, given proximity to major roads and the railroad.
- b. Engineering construction plans shall be submitted for review and approval addressing all public improvements including but not limited to:
    - A. Street improvements per the City minor arterial standards identified by the City's 2011 Transportation Systems Plan (TSP) along the Gable Road frontage. Shall include "Small" street trees as defined by Chapter 17.72 SHMC. Tree location shall also comply with requirements per 17.72.035(2)(d)-(l).

Shall include street lights to illuminate Gable Road accesses (including emergency access) unless existing street lighting is determined to provide adequate illuminance. Plans shall show existing lighting.



The City *recommends* the applicant develop the its Gable Road frontage to the City's 2019 Riverfront Connector Plan standards instead of the TSP standard. City cannot require this however.

Regardless of whether or not the applicant designs to the TSP or Riverfront Connector standard, the design shall include a center turn lane / median.

- B. Emergency access from Gable Road for the off-street parking lot just west of buildings 1 and 4. Shall include bollards with locks approved by the Fire Marshall.
- C. As per condition 6 in regards to utility connection, as applicable.
- D. Where a City (public) utility (water, sanitary sewer and storm sewer) ends and transitions to a private system within the site, there shall be a physical and reasonably observable demarcation as approved by the City of this public to private transition point.
- E. Locations of all public utilities within that site shall be easily accessible for maintenance and repair.
- F. Water main shall be extended along the Gable Road right-of-way to a point that is at least half way across the subject property's entire frontage.
- c. An erosion control plan shall be reviewed and approved by City Engineering to prevent erosion of any new soil materials.
- d. A drainage plan certified by a registered professional engineer shall be reviewed and approved by City Engineering to address water quality to protect surrounding wetlands, water quantity (e.g., to prevent ponding and for storm water retention if needed) and conveyance of storm water. Downstream system impacts shall be evaluated and historical drainage shall not be negatively impacted. Drainage plan shall comply with City, State and Federal standards.

Plans shall also specifically demonstrate compliance with Chapter 17.40 SHMC (e.g., addressing negative impacts to wetlands such as > water volume, pollutants, and discharge velocity/erosion).

- e. Proof of US Army Corps of Engineers and the Oregon Division of State Lands of approval of proposed wetlands reductions/impacts as shown on the plans for this proposal.
- f. Wetland/protection zone management plan by a qualified environmental professional to protect all wetlands and associated protections zones to be preserved. Protection both during construction and after shall be included. Shall include signage in sufficient locations to alert residents, visitors, the public and contractors of the sensitive lands beyond and limits of allowed impact. Signage must be permanent after construction.



- g. A crime prevention/lighting plan shall be submitted for review and approval per the requirements of SHMC 17.96.180(9). This plan shall demonstrate adequate illumination of “areas vulnerable to crime,” while being sensitive to illumination impacts that could negatively wetlands. Light fixture types and details, and photometrics shall be included.
  - h. The applicant shall provide easement legal descriptions and illustrative exhibits per City specifications for all proposed public infrastructure within the subject property. Legal descriptions shall be based on approved engineering construction plans.
  - i. Applicant shall pay their proportional share, based on project impact, of project L01 in Table 7-7 in the City’s 2011 TSP (as explained herein). The project cost shall be based on the estimated cost in the TSP with inflation adjustment to current value. Note that 29.5% of this cost is System Development Charge creditable.
3. The following shall be required **prior to** Certificate of Occupancy by the City Building Official:
    - a. All improvements necessary to address the requirements herein, and in accordance with approved plans, shall be in place.
    - b. Easements for all proposed public infrastructure within the subject property shall be recorded as approved by the City.
    - c. The multiple lots / parcels that make up the property shall be consolidated as one for development purposes with a land use restriction recorded on the deed of the property as approved by the City. As an alternative, the applicant could replat the property into a single parcel.
  4. The address used for the former Columbia County transfer station—2285 Gable Road—shall no longer be used for this site.
  5. Service facilities such as gas meters and air conditioners which would otherwise be visible from a public street, customer or resident parking area, any public facility or any residential area shall be screened, **regardless if such screening is absent on any plan reviewed by the City. This includes but is not limited to ground mounted, roof mounted or building mounted units.** See SHMC 17.72.110(2).
  6. Trash enclosures that will have a drain shall be covered and include connection to the sanitary sewer system. They shall be designed to prevent storm water from entering the sanitary sewer system.
  7. Any refuse container or refuse collection area visible from a public street, parking lot, residential or commercial area, or any public facility (e.g., school or park) shall be screened or enclosed from view by a solid wood (or otherwise sight-obscuring) fence, masonry wall or evergreen hedge.



8. Any artificial lighting of the site shall be designed such that there will be no glare into nearby public rights-of-way or residences.
9. Long term bicycle parking intended for residents shall be covered and meet the provisions of the Development Code.
10. Any new sign requires a sign permit prior to installation, pursuant to Chapter 17.88 SHMC.
11. Any new utilities shall be underground. Existing overhead utilities may remain above ground provided there are no new poles.
12. Disabled person parking space(s) shall comply with local, State, and Federal standards.
13. Any requirement of the Fire Marshall as it applies to this proposal shall be met.
14. Wetlands. Prior to any land clearing, alteration, or physical construction (other than survey work or environmental testing) on the subject property(ies), the property owner and developer, if any, shall obtained all required federal, state, and local authorizations, permits and approvals for the proposed development, including any proposed use, or alteration of the site, including also any off-site improvements. Owner/developer shall be solely responsible for obtaining all approvals, permits, licenses, insurance, and authorizations from the responsible federal, state and local authorities, or other entities, necessary to use the property in the manner contemplated, including all authorizations necessary to perform land clearing, construction and improvement of property in the location and manner contemplated. This provision includes, specifically, a permit or statement from the National Marine Fisheries Service and/or Fish and Wildlife Service that owner's proposed use and/or development will not take or harm any endangered or threatened species as that term is defined in applicable federal statutes and administrative rules. The city of St. Helens has no duty, responsibility or liability for requesting, obtaining, ensuring, or verifying owner/developer's compliance with the applicable state and federal agency permit or approval requirements. Any permit or authorization granted by the city, including any exemption, exception, permit, approval or variance pursuant to the Community Development Code (SHMC Title 17) shall not in any way be interpreted as a waiver, modification, or grant of any state or federal agency permits or authorizations or permission to violate any state or federal law or regulation. Owner/developer shall be held strictly liable, and shall hold the city of St. Helens harmless for administrative, civil and criminal penalties for any violation of federal and state statutes, including but not limited to the Clean Water Act, Endangered Species Act and regulations implementing such laws.
15. No plan submitted to the City for approval shall contradict another.
16. Owner/applicant and their successors are still responsible to comply with the City Development Code (SHMC Title 17).



**Attachment(s):**

- Plan set received Aug. 16, 2019
- MWA Architects Memorandum dated and received August 14, 2019
- 2011 TSP Minor Arterial Cross Section
- 2019 Riverfront Connector Gable Road Cross Section
- Traffic Impact Analysis from Kittelson and Associates dated June 27, 2019 (w/o appendices)

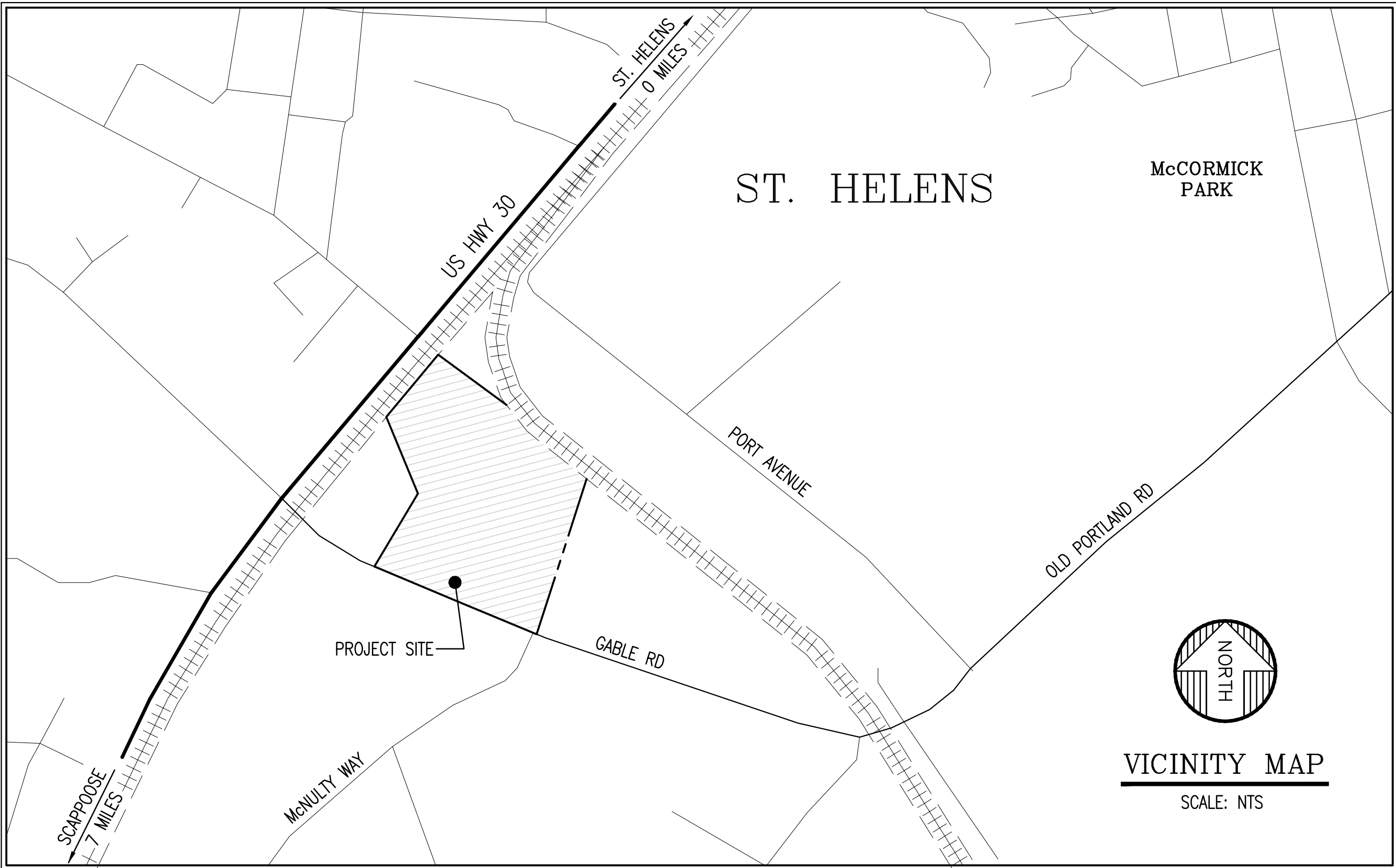


MWA ARCHITECTS INC.

CDP ST. HELENS HOUSING

SCHEMATIC DESIGN - SUMMER 2019

ST. HELENS, OREGON



DEFERRED SUBMITTALS

PLANS AND SPECIFICATIONS REQUIRED FOR THE WORK OF SPECIALTY TRADES MAY BE SUBMITTED AS DEFERRED SUBMITTALS IN ACCORDANCE WITH OSSC SECTION 107.3.4.2. THE FOLLOWING SPECIALITY TRADES WILL BE DESIGN-BUILD AND THEREFORE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR WILL BE OBTAINING SEPARATE PERMITS FOR THIS WORK AND RESPONSIBLE FOR ANY NECESSARY DESIGN AND/OR ENGINEERING TO MEET THE APPLICABLE CODE REQUIREMENTS:

- ELECTRICAL
- MECHANICAL
- PLUMBING
- FIRE SPRINKLERS PER NFPA 13

DRAWING INDEX

DRAWING NUMBER	DESCRIPTION
D-2645-G-1	VICINITY MAP, NOTES & DRAWING INDEX
D-2645-C-1	EXISTING CONDITIONS
D-2645-C-2	CLEARING AND DEMOLITION ESC PLAN
D-2645-C-3	PROPOSED SITE (RESERVED)
D-2645-C-4	GRADING PLAN
D-2645-C-5	FILL PROFILES (RESERVED)
D-2645-C-6	UTILITY PLAN
D-2645-C-7	STORMWATER PLAN
D-2645-C-8	STORM PROFILES (RESERVED)
D-2645-D-1	ESC DETAILS
D-2645-D-2	CIVIL DETAILS
D-2645-D-3	CIVIL DETAILS (RESERVED)

PROJECT TEAM

PROJECT MANAGER/OWNER

COMMUNITY DEVELOPMENT PARTNERS  
126 NE ALBERTA STREET, SUITE 202, PORTLAND OR 97211  
PHONE: (503)-756-0505  
CONTACT: FRANCE MANALAYSAY FITZPATRICK  
france@communitydevpartners.com

ARCHITECT

MWA ARCHITECTS INC.  
70 NW COUCH ST # 401, PORTLAND, OR 97209  
PHONE: (503)-416-8126  
CONTACT: DIANA MOOSMAN, AIA, LEED AP  
dmoosman@mwaarchitects.com

ENGINEER

LOWER COLUMBIA ENGINEERING, LLC  
58640 McNULTY WAY, ST. HELENS, OREGON 97051  
PHONE: (503)-366-0399  
CONTACT: ANDREW NIEMI, P.E.  
andrew@lowercolumbiaengr.com

LANDSCAPE ARCHITECT

ECOTONE  
PHONE: (503)-278-2536  
522 N. THOMPSON ST., SUITE 4, PORTLAND, OR 97227  
CONTACT: DANIEL EDWARDS RLA, LEED AP  
daniel@ecotone-env.com

GEOTECHNICAL ENGINEER

GEO CONSULTANTS NORTHWEST, INC.  
2839 SE MILWAUKIE AVENUE, PORTLAND, OR 97202  
PHONE: (503)-616-9425  
CONTACT: DAVID K. RANKIN, CEG LHG, LEG, QE  
david.rankin@gcnmail.com

NEPA LEAD

TURNSTONE ENVIRONMENTAL CONSULTANTS  
1800 NW LUCY REEDER RD, PORTLAND, OR 97231  
PHONE: (503)-283-5338  
CONTACTS: JOE BETTIS, WPIT  
joe@turnstoneenvironmental.com

GENERAL NOTES

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE O.S.S.C., A.I.S.C. AND A.C.I. CODE.
- ALL CONCRETE USED IN THE CONSTRUCTION OF THE BUILDING FOUNDATIONS SHALL ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AFTER 28 DAYS. ALL EPOXY SHALL BE SPECIAL INSPECTED.
- ALL REINFORCING STEEL SHALL CONFORM TO ASTM A615 GRADE 60, BILLET STEEL DEFORMED BARS.
- ALL REINFORCING SHALL BE SPICED AND/OR BENT TO FULLY DEVELOP THE CAPACITY OF THE BAR (44 x BAR DIAMETER FOR 3,000 PSI CONCRETE).
- CONTRACTOR IS RESPONSIBLE FOR PROPERLY LOCATING ALL SPECIFIED ANCHOR BOLTS. ALL DOOR OPENINGS SHALL BE VERIFIED.
- ALL DRAWINGS MUST BE APPROVED FOR CONSTRUCTION PRIOR TO ANY WORK BEGINNING.
- ALL FOUNDATIONS SHALL BE CARRIED DOWN TO THE ELEVATIONS SHOWN OR SUITABLE UNDISTURBED SOIL, WHICHEVER IS LOWER, OR PLACED ON ENGINEERED FILL PER NOTE 8.
- ANY FILL MATERIAL UNDER SLAB AND FOUNDATIONS SHALL BE 3/4" MINUS CRUSHED ROCK, PLACED IN LAYERS NOT EXCEEDING 8" IN DEPTH AND COMPACTED TO 95% OF THE MAXIMUM DRY DENSITY. ANY SLOPES SHALL BE 2 HORIZONTAL UNITS FOR 1 VERTICAL UNIT MAXIMUM. COMPACTION TESTING IS REQUIRED FOR ANY FILL OVER 12" THICK.
- ALL BACK FILL SHALL BE EVENLY PLACED IN LAYERS NOT EXCEEDING 8" IN DEPTH AND COMPACTED TO 95% OF THE MAXIMUM DRY DENSITY. REMOVE ANY STANDING WATER PRIOR TO BACK FILLING.
- VERIFY WITH ALL PARTIES THAT ALL APPROPRIATE UTILITIES HAVE BEEN INSTALLED PRIOR TO EACH PHASE OF WORK.
- ALL CONCRETE MIX POURED IN A NON-CONTROLLED ENVIRONMENT SHALL CONTAIN 5% AIR ENTRAINMENT PER ASTM C260. ALL WALKING SURFACES SHALL RECEIVE A SLIP RESISTANT SURFACE.
- ALL SITE DESIGN ISSUES, INCLUDING PROPER DRAINAGE AND VERIFICATION OF AN ALLOWABLE SOIL BEARING PRESSURE OF 1,500 PSF AND ANY NECESSARY SLOPE STABILITY ANALYSIS ARE THE RESPONSIBILITY OF OTHERS. CONTACT ENGINEER WITH ANY UNCERTAINTIES PRIOR TO PROCEEDING.
- ALL LIFE SAFETY, BUILDING DESIGN AND CODE REVIEW ISSUES ARE BY OTHERS.
- ANY UNCERTAINTIES SHALL BE ADDRESSED PRIOR TO PROCEEDING, LOWER COLUMBIA ENGINEERING IS NOT RESPONSIBLE FOR THE PROPER IMPLEMENTATION OF THE SPECIFICATIONS CONTAINED ON THESE DRAWINGS.
- DOORS AND ASSOCIATED FRAMING ARE BY OTHERS.
- ALL WEATHER PROOFING AND CODE COMPLIANCE ISSUES ARE THE RESPONSIBILITY OF THE CONTRACTOR/OWNER.
- TOPOGRAPHICAL INFORMATION SHOWN DOES NOT CONSTITUTE LEGAL LAND SURVEY, ALL ELEVATIONS ARE IN LOCAL DATUM, NOT CORRECTED TO NAVD88.

UTILITY LOCATES

(48 HOUR NOTICE PRIOR TO EXCAVATION)

OREGON LAW REQUIRES YOU TO FOLLOW THE RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH 952-001-0100. (YOU MAY OBTAIN COPIES OF THE RULES FROM THE CENTER BY CALLING 503 246 1987.)

ONE CALL SYSTEM.....1 800 332 2344

ON LINE .... <http://www.callbeforeyoudig.org/>

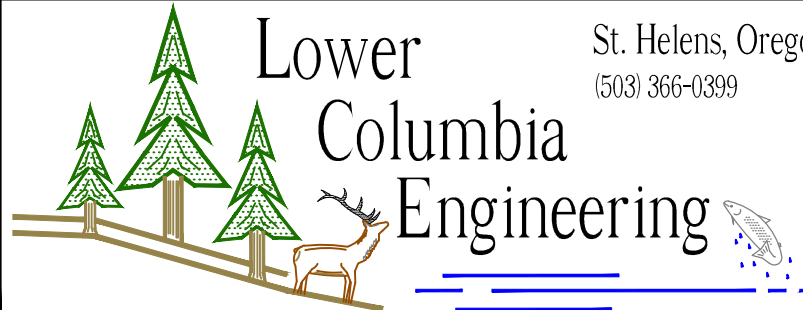
THE PUBLIC WORKS MAINTENANCE SUPERVISOR (NEAL SHEPPEARD) 503-397-3532 MUST BE NOTIFIED 48 HOURS IN ADVANCE TO COORDINATE ANY TAPS AND/OR WATER VALVE OPERATION. THE CONTRACTOR IS NOT ALLOWED TO OPERATE ANY WATER VALVES CONTROLLING FLOW TO NEW PIPING FROM CITY'S POTABLE WATER SYSTEM.

A CITY PERMIT IS REQUIRED TO WORK IN THE PUBLIC R.O.W.

DATE: 06/19/19  
REVISED PRINT  
VOID ALL PREVIOUS

DATE: 06/10/19  
PRELIMINARY  
NOT  
FOR CONSTRUCTION

REV.	REVISION RECORD	DATE
A	GENERAL NOTES UPDATE	06/19/19

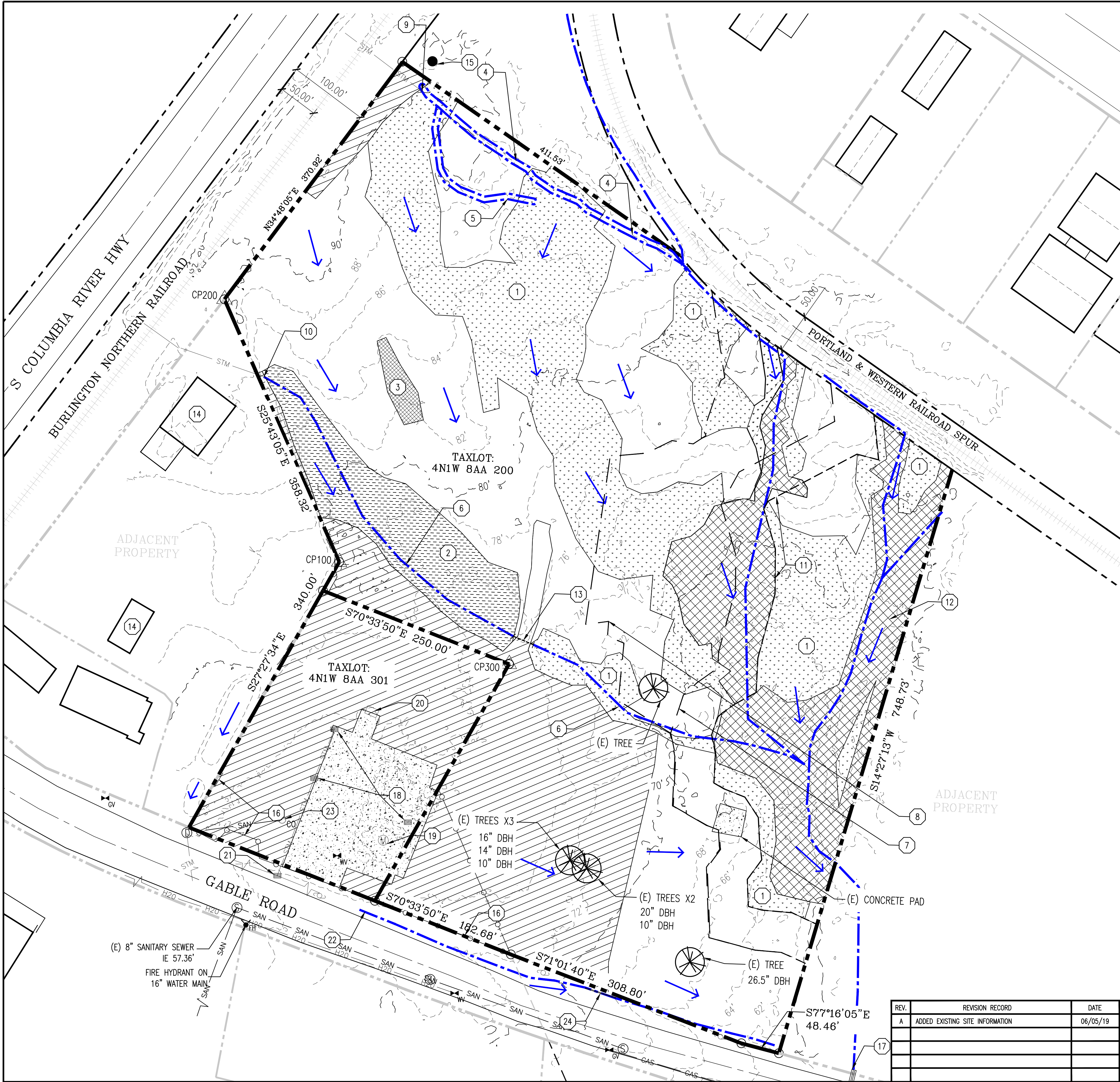


St. Helens, Oregon  
(503) 366-0399

PROJ. NO.	2888	VICINITY MAP, NOTES & DRAWING INDEX
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY	MWA ARCHITECTS INC.	SHEET
FILE	D-2888-G-1-A	DATE 06/05/19

G-1



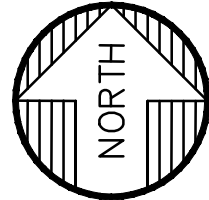


KEYED NOTES	
1	WETLAND "A" (3.51 ACRES TOTAL) 2.2 ACRES TO BE FILLED
2	WETLAND "B" (0.74 ACRES) TO BE FILLED
3	WETLAND "C" (0.06 ACRES) TO BE FILLED
4	EXISTING DITCH "A"
5	EXISTING DITCH "B"
6	EXISTING DITCH "C"
7	CITY OF ST. HELENS UPLAND PROTECTION ZONE
8	FEDERAL WETLAND SETBACK
9	EXISTING 24" HDPE STORM LINE
10	EXISTING 24" CMP STORM LINE
11	EXISTING WETLANDS OF LOCAL SIGNIFICANCE (MC-22)
12	EXISTING WATERWAY AREA
13	EXISTING CULVERT
14	EXISTING BUILDING
15	EXISTING BILLBOARD POLE
16	EXISTING GATE
17	TWO 24" CULVERTS DRAINING TO MCNULTY CREEK
18	EXISTING ABANDONED CATCH BASIN
19	EXISTING ABANDONED MANHOLE
20	EXISTING ABANDONED TREATMENT FACILITY
21	EXISTING STORMWATER CATCH BASIN
22	EXISTING DITCH ALONG GABLE ROAD
23	EXISTING ABANDONED CLEAN OUT
24	EXISTING CULVERT IN DITCH

LEGEND	
CO	CLEAN OUT
(E)	EXISTING
(N)	NEW
EOG	EDGE OF GRAVEL
EOA	EDGE OF ASPHALT
△ CP100	CONTROL POINT
⊙ FH	FIRE HYDRANT
⊙	PROPERTY CORNER
⊙ XX.XX'	POINT ELEVATIONS
⊙	CATCH BASIN
CO	CLEAN OUT
⊙	STORM SEWER MANHOLE
⊙	SANITARY SEWER MANHOLE
⊙	UNKNOWN ABANDONED MANHOLE
⊙ FH	FIRE HYDRANT
⊙ WW	WATER VALVE
⊙ GV	GAS VALVE
⊙	POWER POLE
⊙	WETLAND "A" (3.51 ACRES)
⊙	WETLAND "B" (0.74 ACRES)
⊙	WETLAND "C" (0.06 ACRES)
⊙	WATERWAY AREAS
⊙	EXISTING FILL
⊙	CONCRETE
⊙	(E) MAJOR CONTOUR
⊙	(E) MINOR CONTOUR
⊙	WETLAND SETBACK
⊙	GAS LINE
⊙	POWER LINE
⊙	STORM WATER SEWER
⊙	GAS LINE
⊙	PROPERTY LINE
⊙	ADJACENT PROPERTY LINE
⊙	(E) RAILROAD LINE
⊙	(E) FENCE LINE
⊙	CENTER OF ROAD
⊙	EXISTING FLOW PATH
⊙	FLOW DIRECTION ARROW

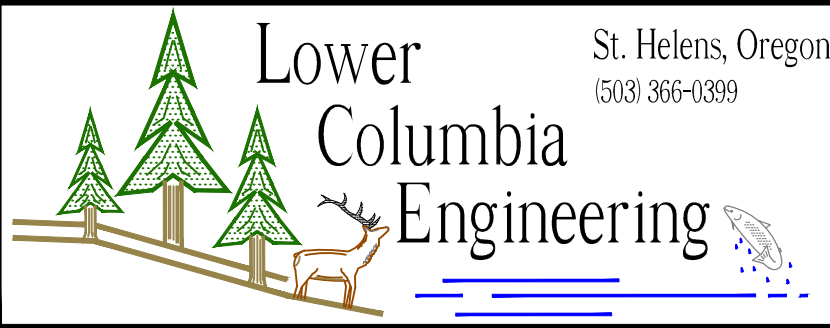
EXISTING CONDITIONS

SCALE: 1" = 60'-0"



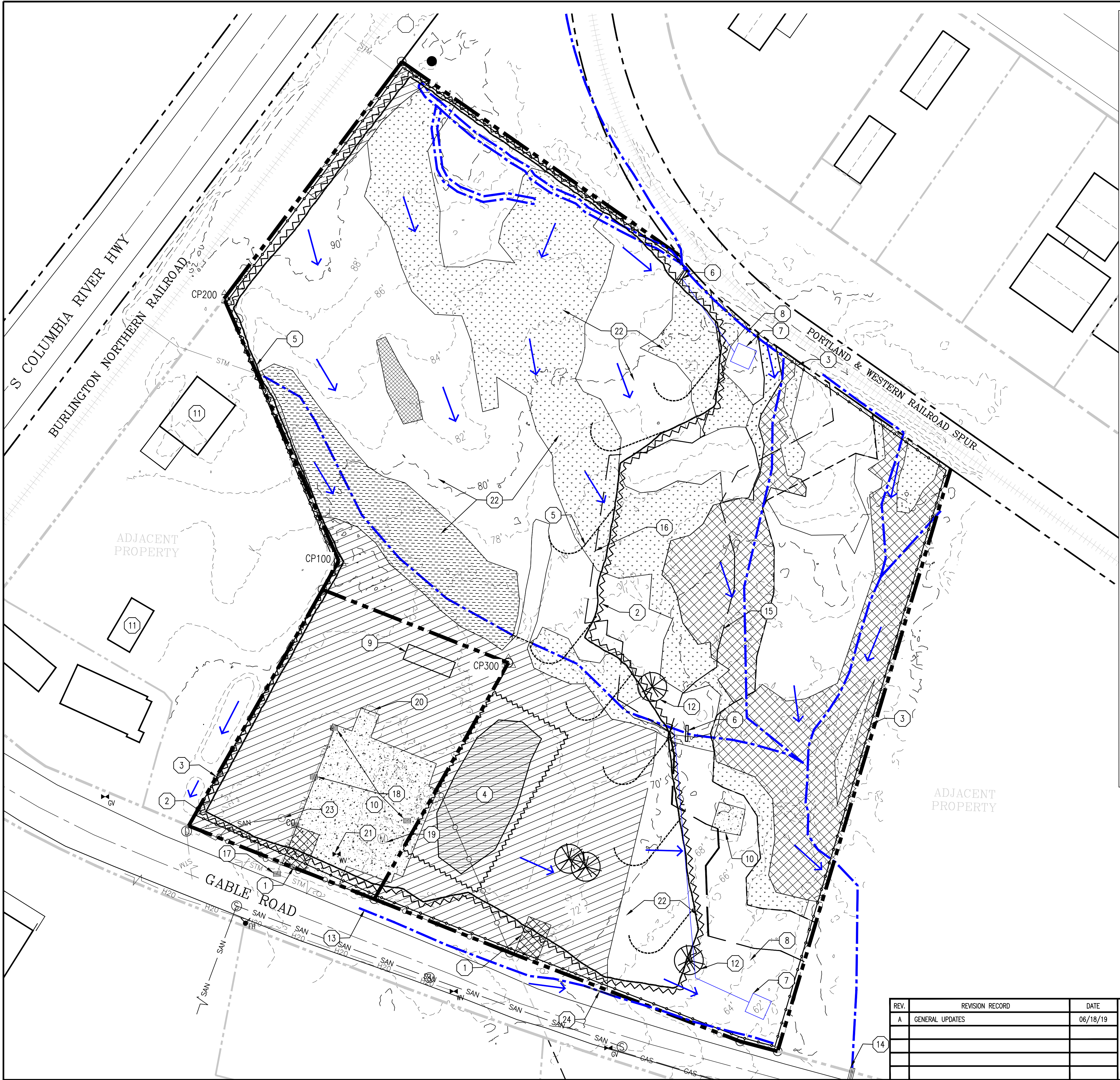
DATE: 06/10/19	DATE: 06/03/19
REVISED PRINT	PRELIMINARY
VOID ALL PREVIOUS	NOT FOR CONSTRUCTION

REV.	REVISION RECORD	DATE
A	ADDED EXISTING SITE INFORMATION	06/05/19



PROJ. NO.	2888	EXISTING CONDITIONS
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY		MWA ARCHITECTS INC.
FILE	D-2888-C-1-A	DATE 05/31/19
		SHEET C-1



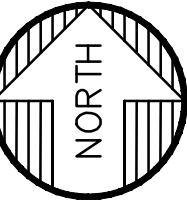


KEYED NOTES	
1	CONSTRUCTION ENTRANCE PER DETAIL 4-5 ON SHEET D-1
2	SEDIMENT FENCE PER DETAIL 4-16 ON SHEET D-1
3	ORANGE CONSTRUCTION FENCE AS NEEDED
4	TEMPORARY CONSTRUCTION STOCKPILE PER DETAIL 4-4 ON SHEET D-1
5	STRAW WATTLE PER DETAIL 4-27 ON SHEET D-1
6	CHECK DAM AS NEEDED
7	PUMP AND FILTER BAG AS NEEDED
8	NATURAL BUFFER ZONE
9	REFUELING AREA
10	CONCRETE TO BE REMOVED
11	EXISTING BUILDING
12	EXISTING TREES TO BE PROTECTED
13	EXISTING DITCH ALONG GABLE ROAD
14	TWO 24" CULVERTS DRAINING TO MCNULY CREEK TO REMAIN
15	CITY OF ST. HELENS UPLAND PROTECTION ZONE
16	FEDERAL WETLAND SETBACK
17	EXISTING STORMWATER CATCH BASIN TO REMAIN
18	EXISTING ABANDONED CATCH BASIN TO BE REMOVED
19	EXISTING ABANDONED MANHOLE TO BE REMOVED
20	EXISTING ABANDONED TREATMENT FACILITY TO BE REMOVED
21	EXISTING WATER VALVE TO BE REMOVED
22	EXISTING SURFACE TO BE GRUBBED AND CLEARED OF VEGETATION PRIOR TO GRADING AND FILLING
23	EXISTING ABANDONED CLEAN OUT TO BE REMOVED
24	EXISTING CULVERT IN DITCH TO REMAIN

LEGEND	
CO	CLEAN OUT
(E)	EXISTING
(N)	NEW
EOG	EDGE OF GRAVEL
EOA	EDGE OF ASPHALT
△ CP100	CONTROL POINT
● FH	FIRE HYDRANT
○ XX.XX'	PROPERTY CORNER
○	POINT ELEVATIONS
■	CATCH BASIN
○	CLEAN OUT
○	STORM SEWER MANHOLE
○	SANITARY SEWER MANHOLE
○	UNKNOWN ABANDONED MANHOLE
● FH	FIRE HYDRANT
● W	WATER VALVE
● GV	GAS VALVE
○	POWER POLE
■	WETLAND "A" (3.51 ACRES)
■	WETLAND "B" (0.74 ACRES)
■	WETLAND "C" (0.06 ACRES)
■	WATERWAY AREAS
■	EXISTING FILL
■	CONCRETE
--- 100' ---	(E) MAJOR CONTOUR
--- 98' ---	(E) MINOR CONTOUR
---	WETLAND SETBACK
---	GAS LINE
---	POWER LINE
---	STORM WATER SEWER
---	PROPERTY LINE
---	ADJACENT PROPERTY LINE
---	(E) RAILROAD LINE
---	(E) FENCE LINE
---	CENTER OF ROAD
---	EXISTING FLOW PATH
---	FLOW DIRECTION ARROW
---	ORANGE CONSTRUCTION FENCE (AS REQUIRED)
---	CENTER OF ROAD
---	SEDIMENT FENCE (REQUIRED)
---	WATTLES (AS NEEDED)
---	CONSTRUCTION ENTRANCE SEE DETAIL 4-5

EROSION AND SEDIMENT CONTROL PLAN

SCALE: 1" = 60'-0"



DATE: 06/19/19	DATE: 06/10/19
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REV.	REVISION RECORD	DATE
A	GENERAL UPDATES	06/18/19

Lower Columbia Engineering  
St. Helens, Oregon  
(503) 366-0399

PROJ. NO.	2888	CLEARING AND DEMOLITION ESC PLAN
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY	MWA ARCHITECTS INC.	SHEET
FILE	D-2888-C-2-A	DATE 06/05/19
		C-2

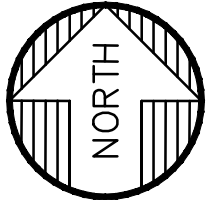




- NOTES:
1. ALL STORM WATER PIPES SHALL HAVE .5% MINIMUM SLOPE AND 18" OF COVER MINIMUM.
  2. ALL HARD SURFACES SHALL HAVE A MINIMUM OF 1.0% SLOPE TOWARDS COLLECTION SYSTEM.
  3. ALL WALKWAYS SHALL HAVE A MAXIMUM SLOPE OF 5.0% IN THE DIRECTION OF TRAVEL AND 2.0% CROSS SLOPE.
  4. PLAN CAN BE PROVIDED IN CAD FORMAT TO ACCOMMODATE CONSTRUCTION STAKING. (NOT ALL DIMENSIONS ARE SHOWN)
  5. LANDSCAPE AREAS SHALL BE SLOPED TOWARDS CATCH BASINS AS REQUIRED TO PREVENT PUDDLES. PROVIDE 2"-6" DRAIN ROCK SURFACE WITHIN 3' OF LANDSCAPE CATCH BASINS TO REDUCE THE POTENTIAL OF BLOCKAGE.
  6. PRIVATE SEWER CONSTRUCTION: ON-SITE BUILDING SEWERS OUTSIDE OF PUBLIC SEWER EASEMENTS ARE PRIVATE AND SHALL BE CONSTRUCTED ACCORDING TO THE 2017 OREGON PLUMBING SPECIALTY CODE.
  7. SEWER SERVICE LATERAL CONNECTION AND PIPING WITHIN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED TO PUBLIC STANDARDS AND ARE SUBJECT TO CONSTRUCTION SERVICES TESTING AND INSPECTION REQUIREMENTS.

LEGEND	
EOG	EDGE OF GRAVEL
EOP	EDGE OF PAVEMENT
EOC	EDGE OF CONCRETE
TOP	TOP OF PAVEMENT
TOC	TOP OF CONCRETE
TO/CURB	TOP OF CURB
(E)	EXISTING
(N)	NEW
CO	CLEAN OUT
⊙	STORM SEWER MANHOLE
⊙	SANITARY SEWER MANHOLE
⊙	NEW CATCH BASIN
FH	FIRE HYDRANT
WV	WATER VALVE
⊙	UTILITY POLE
MM	WATER METER
---	SUBJECT PROPERTY LINE
---	ADJACENT PROPERTY LINE
---	WETLAND SETBACK
---	(E) MAJOR CONTOUR
---	(E) MINOR CONTOUR
---	(N) MAJOR CONTOUR
---	(N) MINOR CONTOUR
---	WETLAND
---	WATERWAY

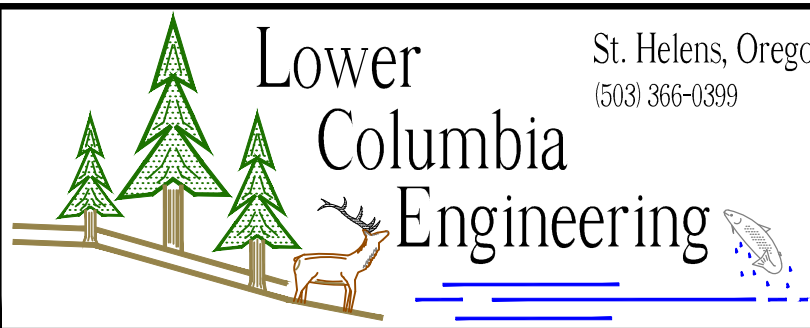
GRADING PLAN  
SCALE: 1" = 60'-0"



DATE: 06/19/19  
REVISED PRINT  
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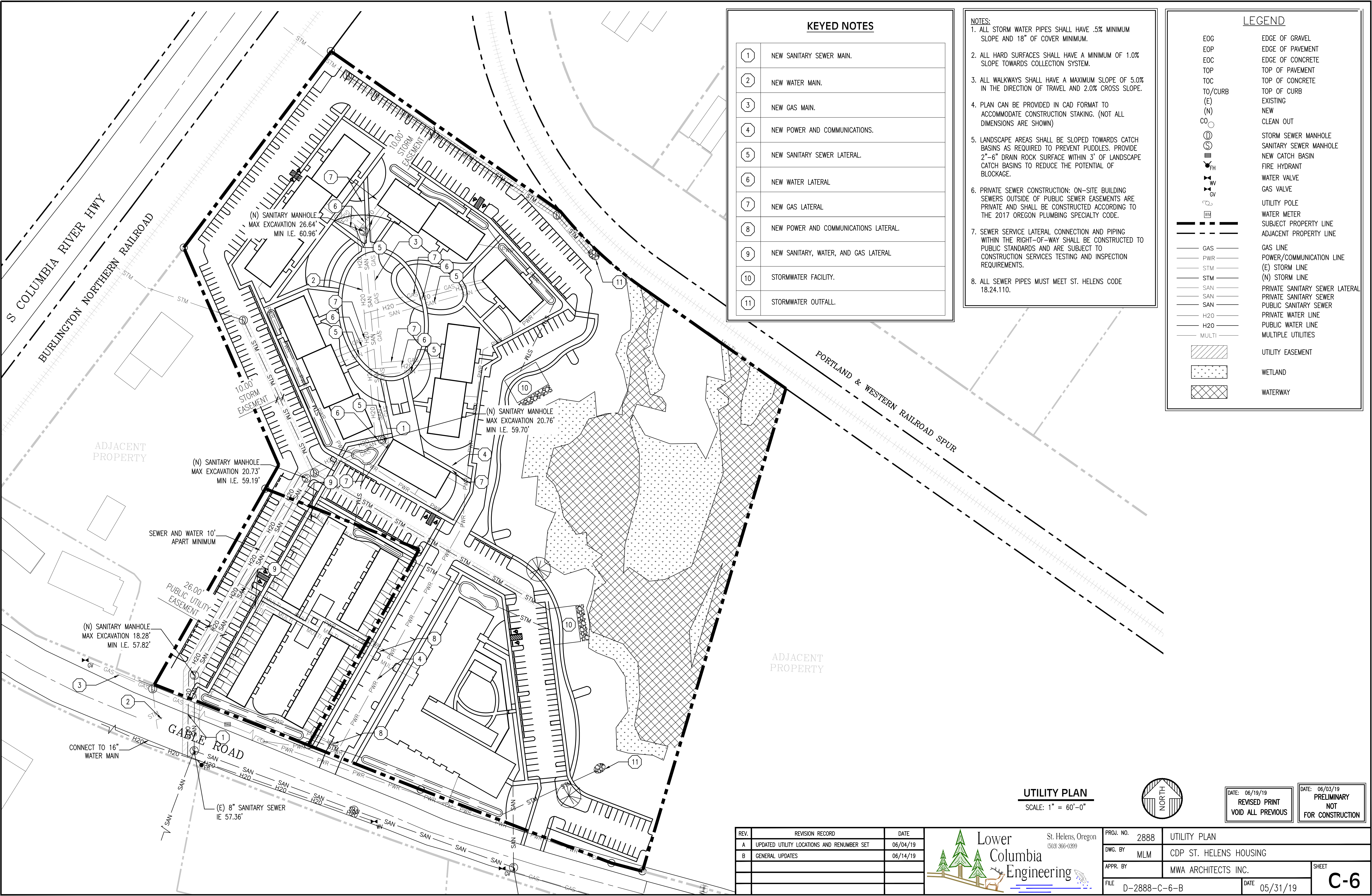
DATE: 06/10/19  
PRELIMINARY  
NOT  
FOR CONSTRUCTION

REV.	REVISION RECORD	DATE
A	GENERAL UPDATES	06/11/19



PROJ. NO.	2888	GRADING PLAN
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY	MWA ARCHITECTS INC.	SHEET
FILE	D-2888-C-4-A	DATE 06/05/19
		C-4





KEYED NOTES

1	NEW SANITARY SEWER MAIN.
2	NEW WATER MAIN.
3	NEW GAS MAIN.
4	NEW POWER AND COMMUNICATIONS.
5	NEW SANITARY SEWER LATERAL.
6	NEW WATER LATERAL
7	NEW GAS LATERAL
8	NEW POWER AND COMMUNICATIONS LATERAL.
9	NEW SANITARY, WATER, AND GAS LATERAL
10	STORMWATER FACILITY.
11	STORMWATER OUTFALL.

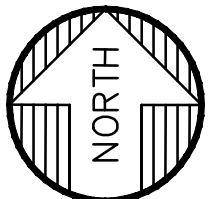
- NOTES:
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  4. PLAN CAN BE PROVIDED IN CAD FORMAT TO ACCOMMODATE CONSTRUCTION STAKING. (NOT ALL DIMENSIONS ARE SHOWN)
  5. LANDSCAPE AREAS SHALL BE SLOPED TOWARDS CATCH BASINS AS REQUIRED TO PREVENT PUDDLES. PROVIDE 2"-6" DRAIN ROCK SURFACE WITHIN 3' OF LANDSCAPE CATCH BASINS TO REDUCE THE POTENTIAL OF BLOCKAGE.
  6. PRIVATE SEWER CONSTRUCTION: ON-SITE BUILDING SEWERS OUTSIDE OF PUBLIC SEWER EASEMENTS ARE PRIVATE AND SHALL BE CONSTRUCTED ACCORDING TO THE 2017 OREGON PLUMBING SPECIALTY CODE.
  7. SEWER SERVICE LATERAL CONNECTION AND PIPING WITHIN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED TO PUBLIC STANDARDS AND ARE SUBJECT TO CONSTRUCTION SERVICES TESTING AND INSPECTION REQUIREMENTS.
  8. ALL SEWER PIPES MUST MEET ST. HELENS CODE 18.24.110.

LEGEND

EOG	EDGE OF GRAVEL
EOP	EDGE OF PAVEMENT
EOC	EDGE OF CONCRETE
TOP	TOP OF PAVEMENT
TOC	TOP OF CONCRETE
TO/CURB	TOP OF CURB
(E)	EXISTING
(N)	NEW
CO	CLEAN OUT
⊙	STORM SEWER MANHOLE
⊙	SANITARY SEWER MANHOLE
⊙	NEW CATCH BASIN
FH	FIRE HYDRANT
WV	WATER VALVE
GV	GAS VALVE
⊙	UTILITY POLE
⊙	WATER METER
---	SUBJECT PROPERTY LINE
---	ADJACENT PROPERTY LINE
---	GAS LINE
---	POWER/COMMUNICATION LINE
---	(E) STORM LINE
---	(N) STORM LINE
---	PRIVATE SANITARY SEWER LATERAL
---	PRIVATE SANITARY SEWER
---	PUBLIC SANITARY SEWER
---	PRIVATE WATER LINE
---	PUBLIC WATER LINE
---	MULTIPLE UTILITIES
---	UTILITY EASEMENT
---	WETLAND
---	WATERWAY

UTILITY PLAN

SCALE: 1" = 60'-0"



DATE: 06/19/19	DATE: 06/03/19
REVISED PRINT	PRELIMINARY
VOID ALL PREVIOUS	NOT FOR CONSTRUCTION

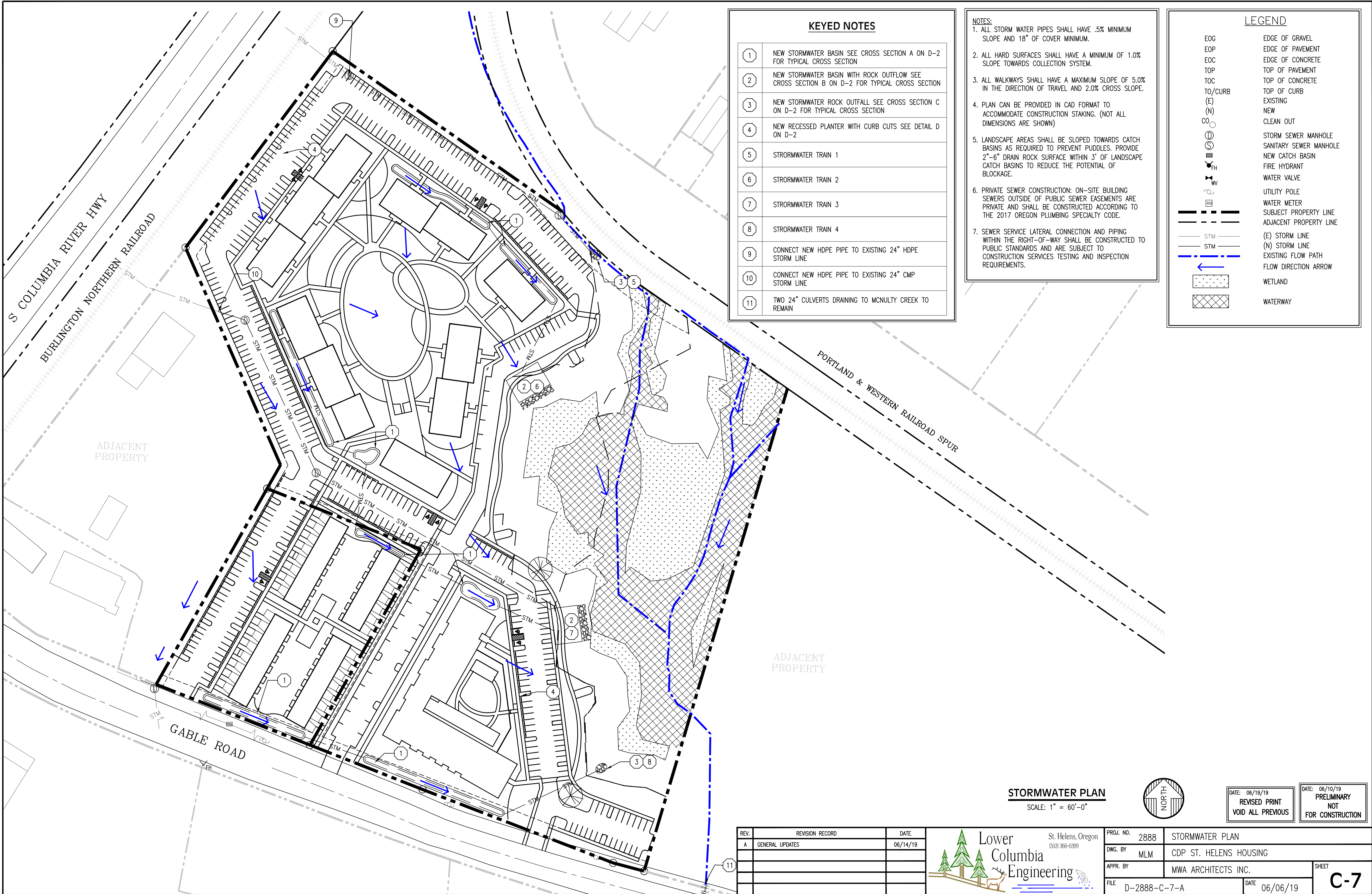
REV.	REVISION RECORD	DATE
A	UPDATED UTILITY LOCATIONS AND RENUMBER SET	06/04/19
B	GENERAL UPDATES	06/14/19

Lower Columbia Engineering

St. Helens, Oregon  
(503) 366-0399

PROJ. NO.	2888	UTILITY PLAN
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY	MWA ARCHITECTS INC.	SHEET
FILE	D-2888-C-6-B	DATE 05/31/19
		C-6





KEYED NOTES

- 1. NEW STORMWATER BASIN SEE CROSS SECTION A ON D-2 FOR TYPICAL CROSS SECTION
- 2. NEW STORMWATER BASIN WITH ROCK OUTFLOW SEE CROSS SECTION B ON D-2 FOR TYPICAL CROSS SECTION
- 3. NEW STORMWATER ROCK OUTFALL SEE CROSS SECTION C ON D-2 FOR TYPICAL CROSS SECTION
- 4. NEW RECESSED PLANTER WITH CURB CUTS SEE DETAIL D ON D-2
- 5. STORMWATER TRAIN 1
- 6. STORMWATER TRAIN 2
- 7. STORMWATER TRAIN 3
- 8. STORMWATER TRAIN 4
- 9. CONNECT NEW HDPE PIPE TO EXISTING 24" HDPE STORM LINE
- 10. CONNECT NEW HDPE PIPE TO EXISTING 24" CMP STORM LINE
- 11. TWO 24" CULVERTS DRAINING TO MCNULTY CREEK TO REMAIN

NOTES:

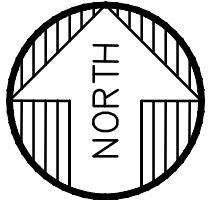
- 1. ALL STORM WATER PIPES SHALL HAVE .5% MINIMUM SLOPE AND 18" OF COVER MINIMUM.
- 2. ALL HARD SURFACES SHALL HAVE A MINIMUM OF 1.0% SLOPE TOWARDS COLLECTION SYSTEM.
- 3. ALL WALKWAYS SHALL HAVE A MAXIMUM SLOPE OF 5.0% IN THE DIRECTION OF TRAVEL AND 2.0% CROSS SLOPE.
- 4. PLAN CAN BE PROVIDED IN CAD FORMAT TO ACCOMMODATE CONSTRUCTION STAKING. (NOT ALL DIMENSIONS ARE SHOWN)
- 5. LANDSCAPE AREAS SHALL BE SLOPED TOWARDS CATCH BASINS AS REQUIRED TO PREVENT PUDDLES. PROVIDE 2"-6" DRAIN ROCK SURFACE WITHIN 3' OF LANDSCAPE CATCH BASINS TO REDUCE THE POTENTIAL OF BLOCKAGE.
- 6. PRIVATE SEWER CONSTRUCTION: ON-SITE BUILDING SEWERS OUTSIDE OF PUBLIC SEWER EASEMENTS ARE PRIVATE AND SHALL BE CONSTRUCTED ACCORDING TO THE 2017 OREGON PLUMBING SPECIALTY CODE.
- 7. SEWER SERVICE LATERAL CONNECTION AND PIPING WITHIN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED TO PUBLIC STANDARDS AND ARE SUBJECT TO CONSTRUCTION SERVICES TESTING AND INSPECTION REQUIREMENTS.

LEGEND

- EOG EDGE OF GRAVEL
- EOP EDGE OF PAVEMENT
- EOC EDGE OF CONCRETE
- TOP TOP OF PAVEMENT
- TOC TOP OF CONCRETE
- TO/CURB TOP OF CURB
- (E) EXISTING
- (N) NEW
- CO CLEAN OUT
- SM STORM SEWER MANHOLE
- SS SANITARY SEWER MANHOLE
- NB NEW CATCH BASIN
- FH FIRE HYDRANT
- WV WATER VALVE
- UP UTILITY POLE
- WM WATER METER
- SL SUBJECT PROPERTY LINE
- AL ADJACENT PROPERTY LINE
- (E) STORM LINE
- (N) STORM LINE
- EF EXISTING FLOW PATH
- FD FLOW DIRECTION ARROW
- WET WETLAND
- WA WATERWAY

STORMWATER PLAN

SCALE: 1" = 60'-0"



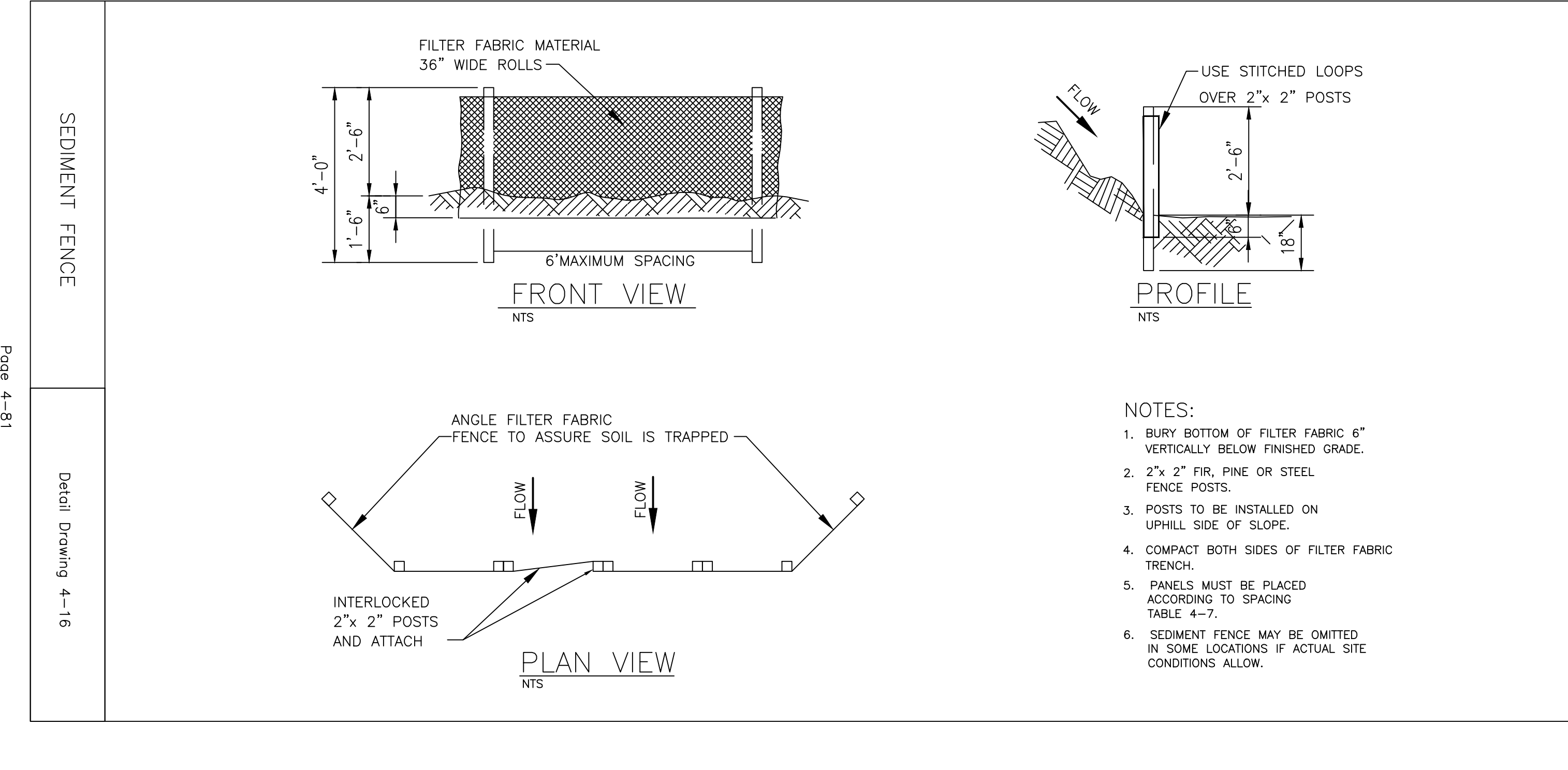
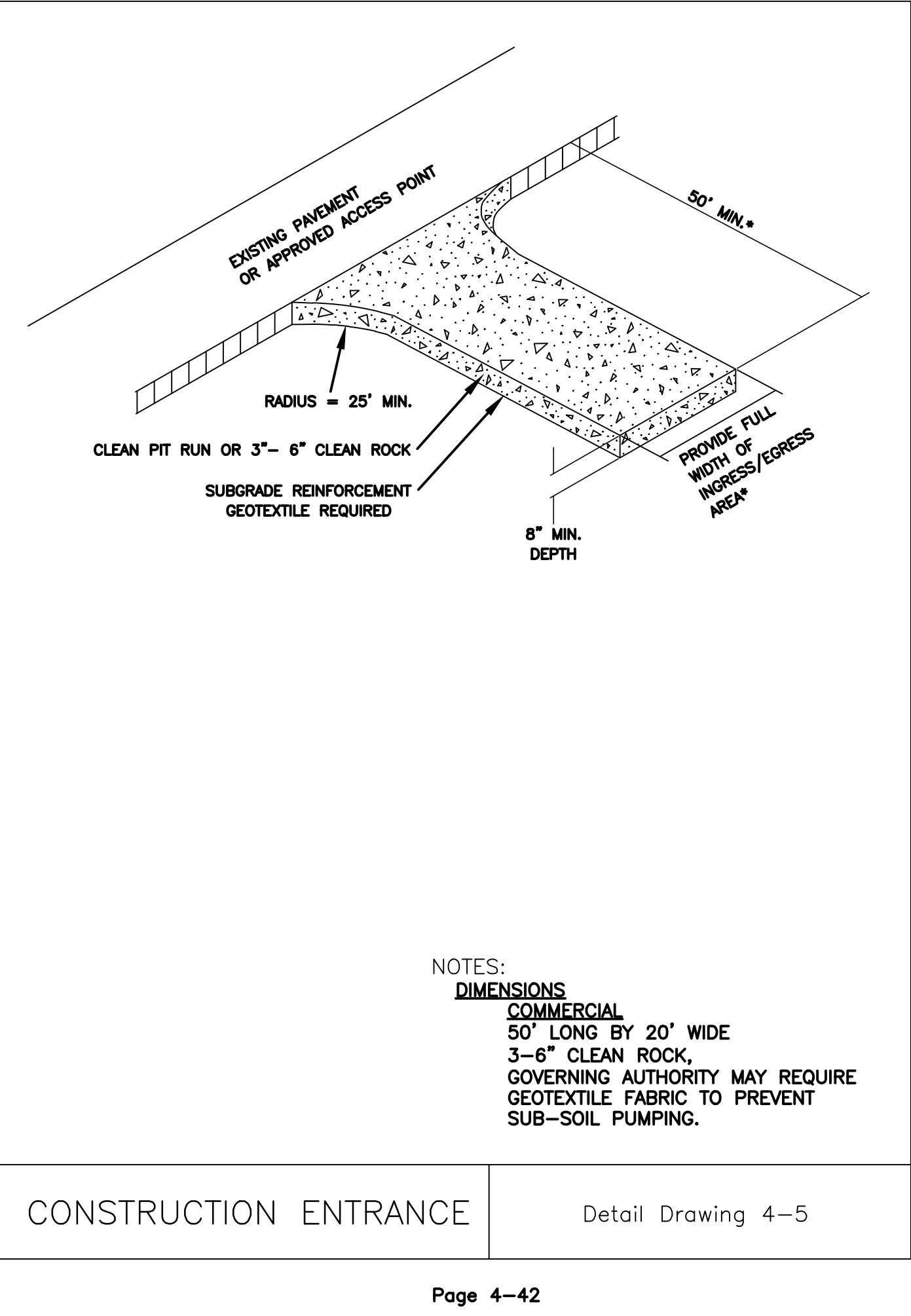
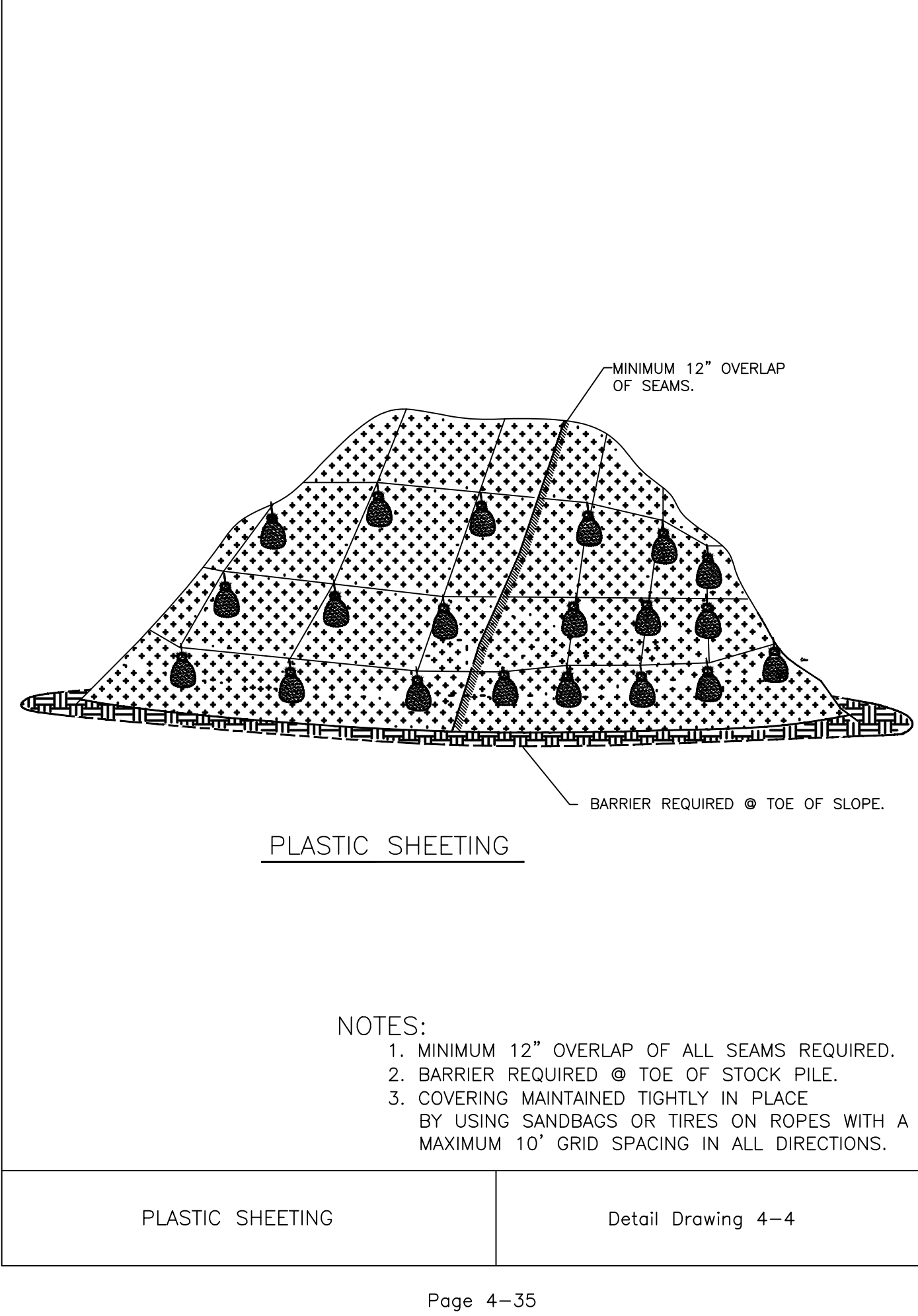
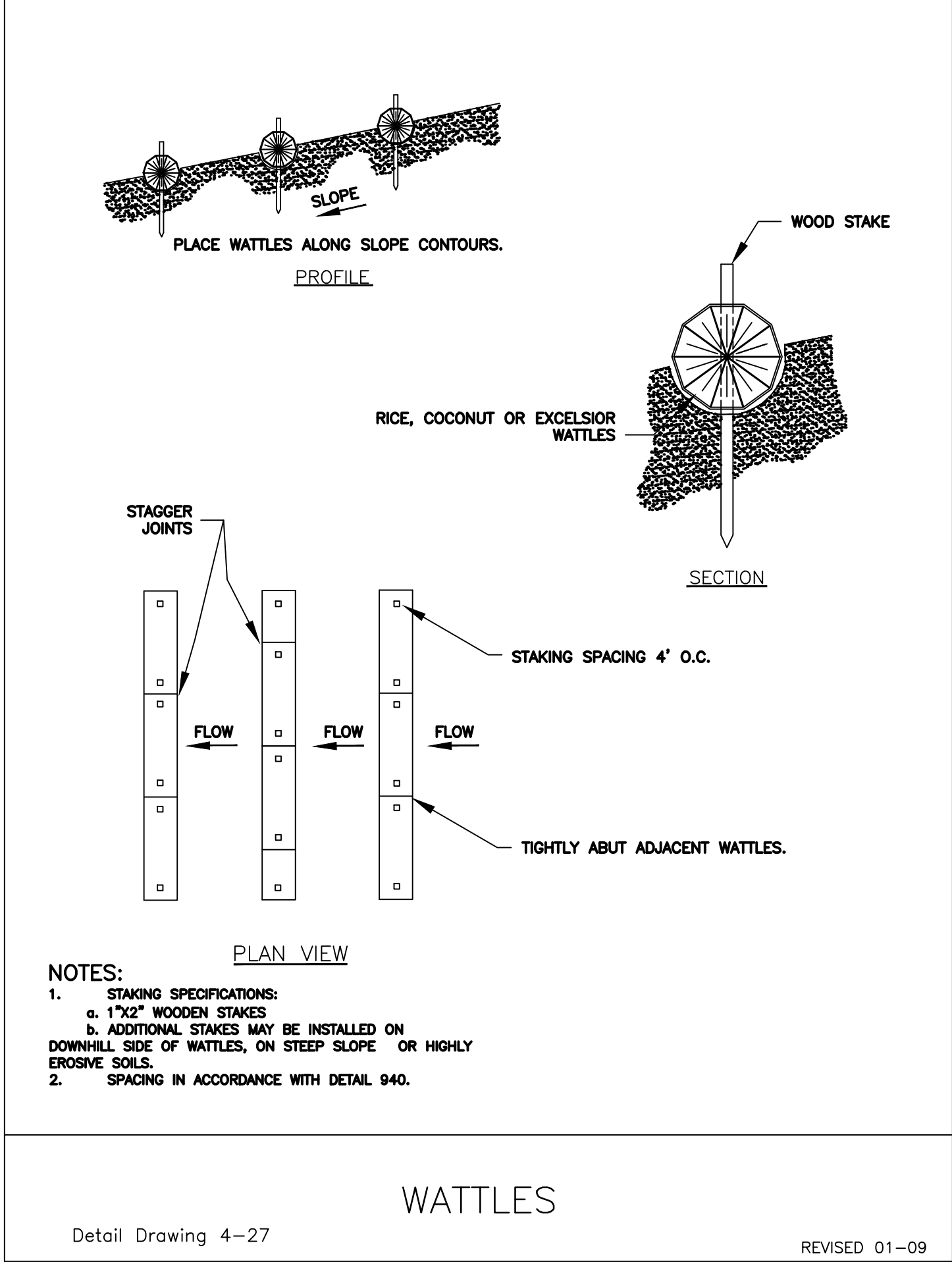
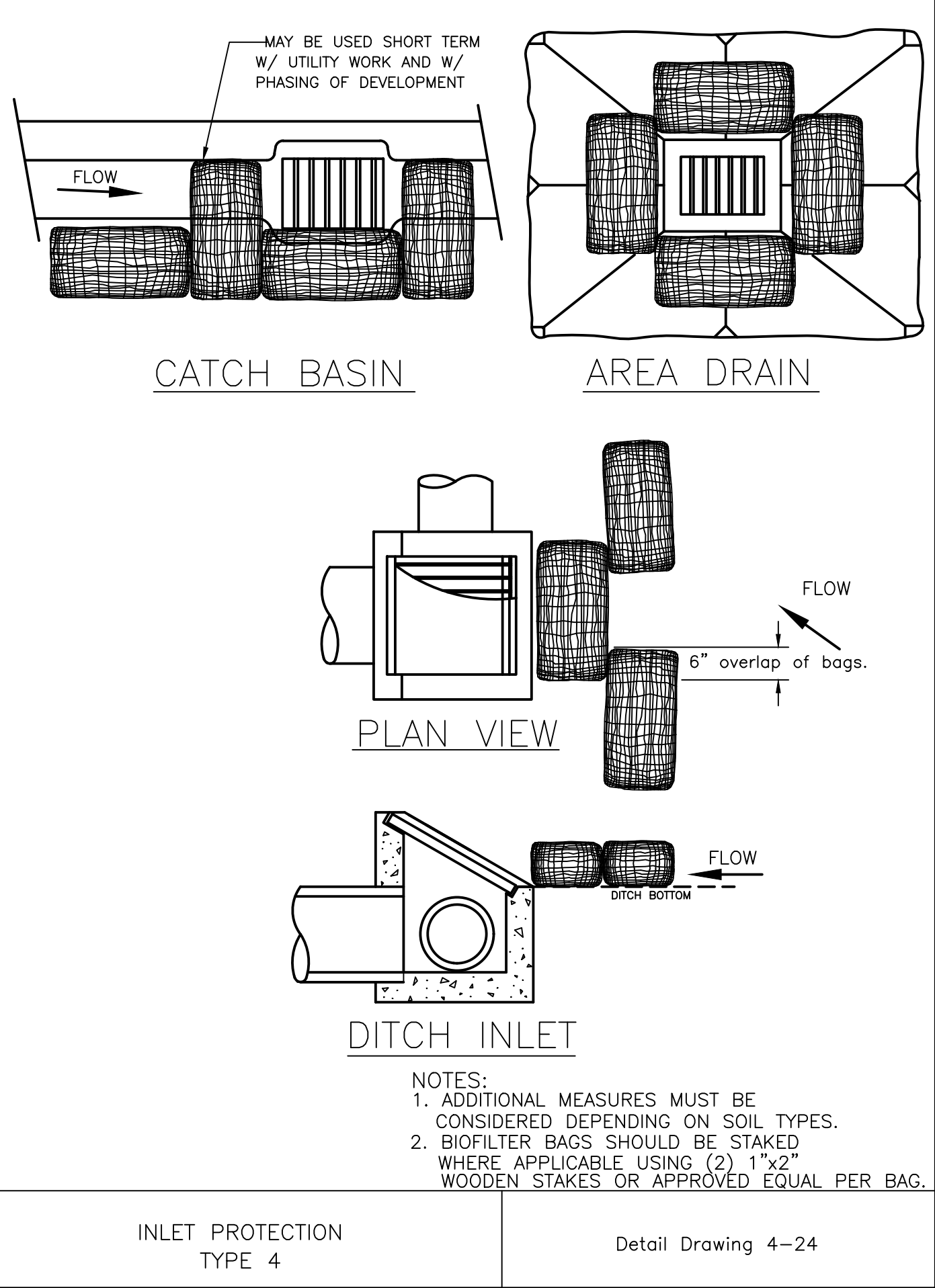
DATE: 06/19/19	DATE: 06/10/19
REVISED PRINT	PRELIMINARY
VOID ALL PREVIOUS	NOT FOR CONSTRUCTION

REV.	REVISION RECORD	DATE
A	GENERAL UPDATES	06/14/19

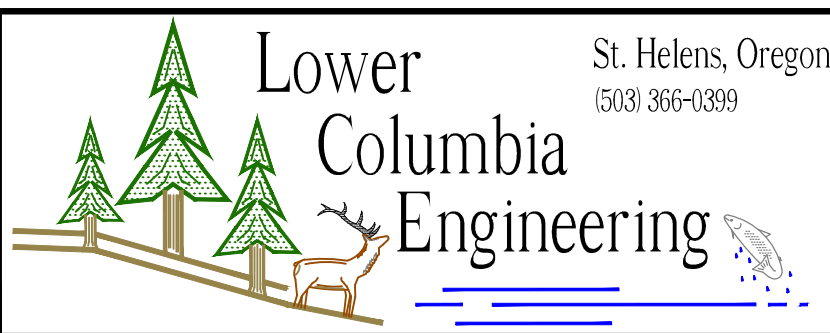
Lower Columbia Engineering  
St. Helens, Oregon  
(503) 366-0399

PROJ. NO.	2888	STORMWATER PLAN
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY	MWA ARCHITECTS INC.	SHEET
FILE	D-2888-C-7-A	DATE 06/06/19
		C-7





REV.	REVISION RECORD	DATE



PROJ. NO.	2888	ESC DETAILS
DWG. BY	MLM	CDP ST. HELENS HOUSING
APPR. BY		MWA ARCHITECTS INC.
FILE	D-2888-D-1	DATE 06/05/19

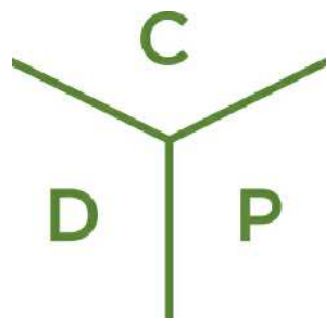
DATE: 06/10/19  
PRELIMINARY  
NOT  
FOR CONSTRUCTION

SHEET  
**D-1**









REVISION NO      DATE


COMMUNITY  
DEVELOPMENT  
PARTNERS  
ST. HELENS  
HOUSING

GABLE ROAD  
ST. HELENS, OR 97051

ISSUANCE  
CONDITIONAL USE

DATE  
08/14/2019

SCALE

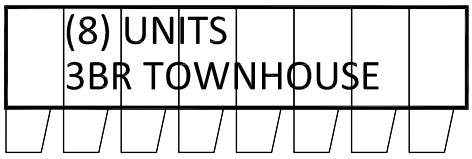
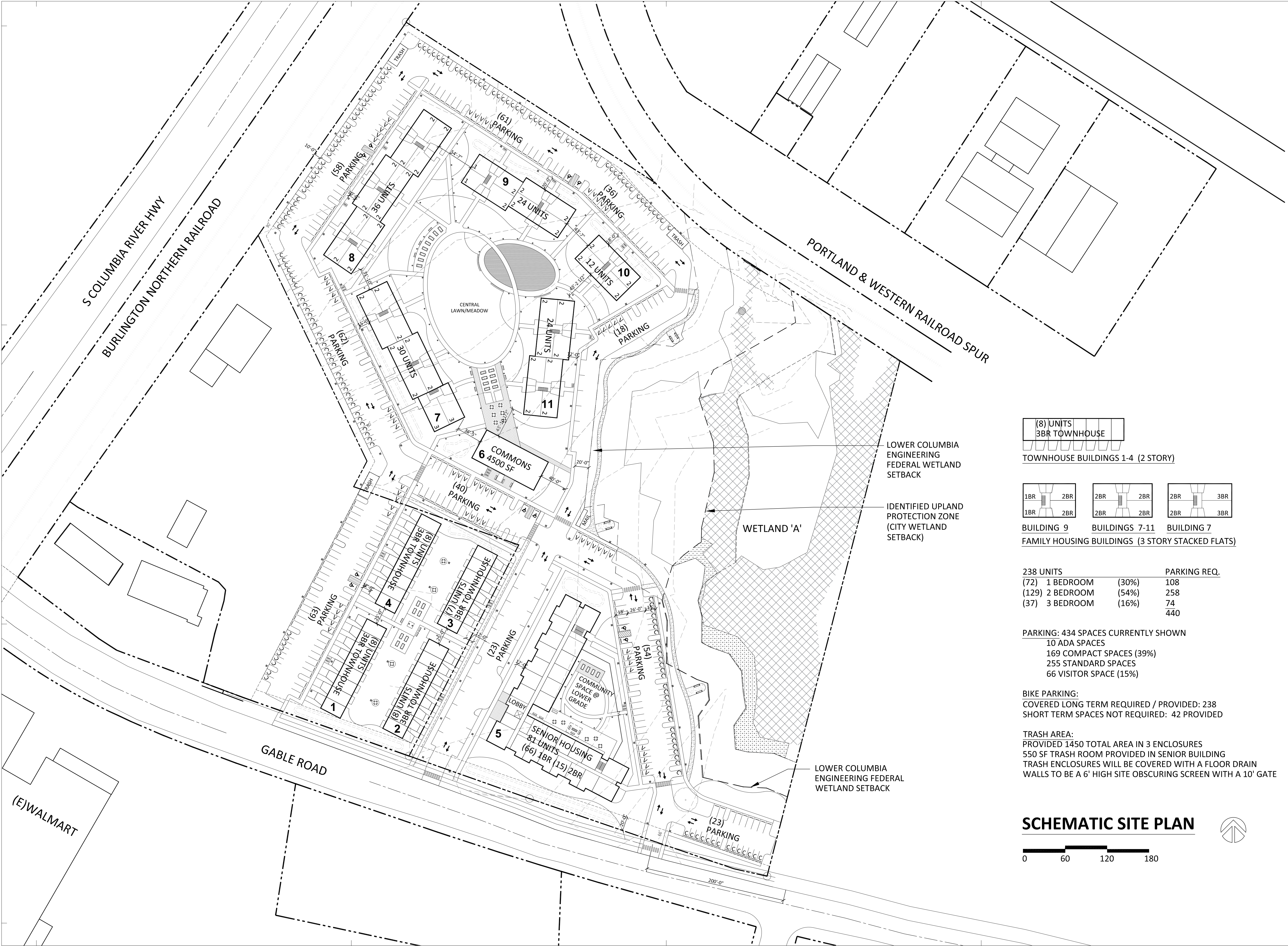
PROJECT NUMBER  
2019103.00

DRAWN BY  
DM

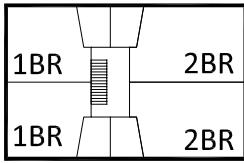
DRAWING TITLE  
SITE PLAN

SHEET NUMBER

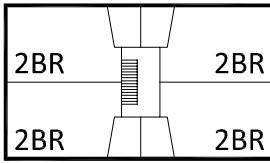
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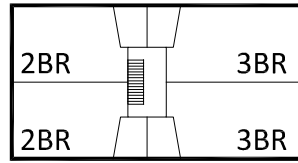
TOWNHOUSE BUILDINGS 1-4 (2 STORY)



BUILDING 9



BUILDINGS 7-11



BUILDING 7

FAMILY HOUSING BUILDINGS (3 STORY STACKED FLATS)

238 UNITS		PARKING REQ.
(72) 1 BEDROOM	(30%)	108
(129) 2 BEDROOM	(54%)	258
(37) 3 BEDROOM	(16%)	74
		440

PARKING: 434 SPACES CURRENTLY SHOWN  
10 ADA SPACES  
169 COMPACT SPACES (39%)  
255 STANDARD SPACES  
66 VISITOR SPACE (15%)

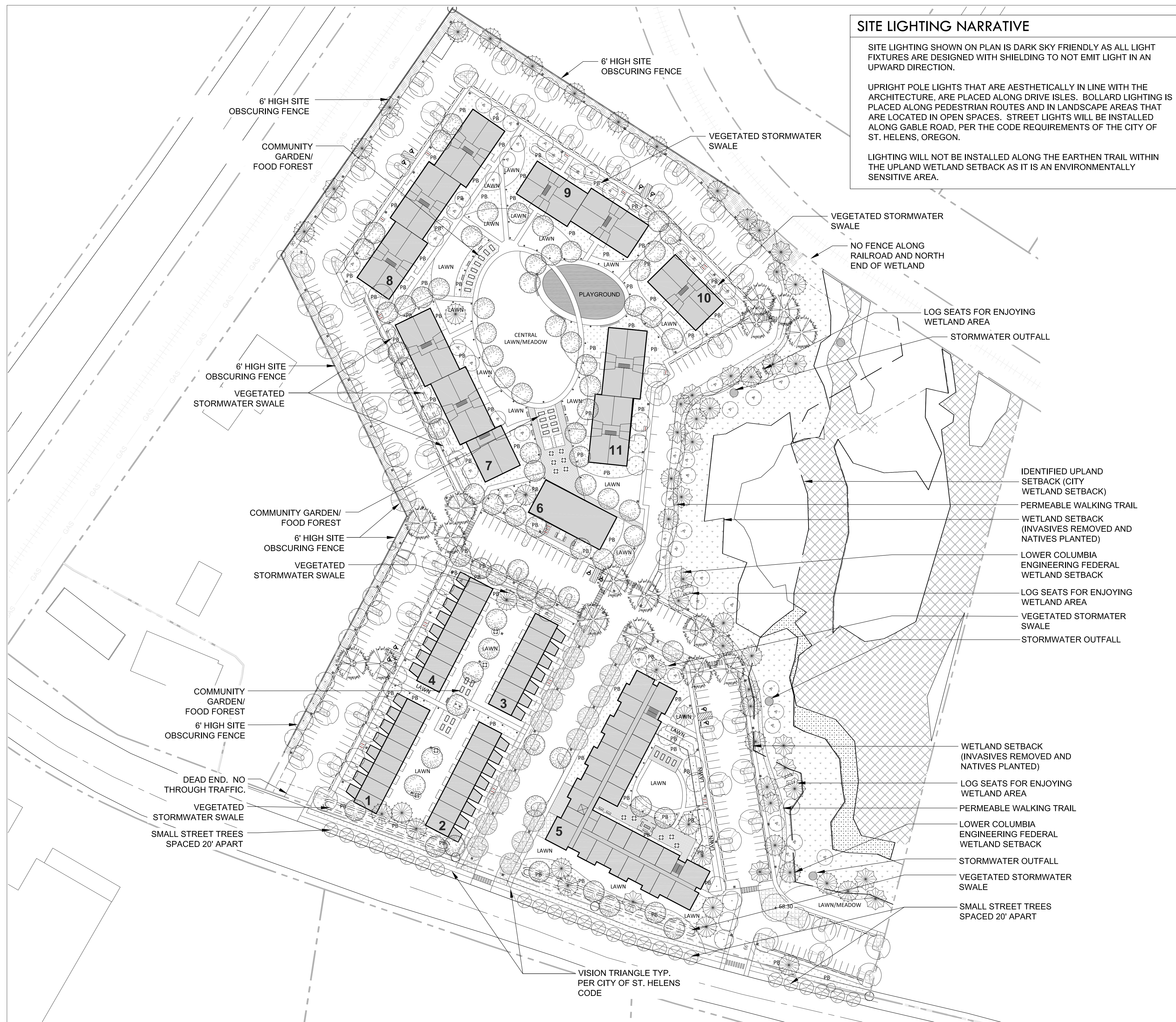
BIKE PARKING:  
COVERED LONG TERM REQUIRED / PROVIDED: 238  
SHORT TERM SPACES NOT REQUIRED: 42 PROVIDED

TRASH AREA:  
PROVIDED 1450 TOTAL AREA IN 3 ENCLOSURES  
550 SF TRASH ROOM PROVIDED IN SENIOR BUILDING  
TRASH ENCLOSURES WILL BE COVERED WITH A FLOOR DRAIN  
WALLS TO BE A 6' HIGH SITE OBSCURING SCREEN WITH A 10' GATE

SCHEMATIC SITE PLAN







### SITE LIGHTING NARRATIVE

SITE LIGHTING SHOWN ON PLAN IS DARK SKY FRIENDLY AS ALL LIGHT FIXTURES ARE DESIGNED WITH SHIELDING TO NOT EMIT LIGHT IN AN UPWARD DIRECTION.

UPRIGHT POLE LIGHTS THAT ARE AESTHETICALLY IN LINE WITH THE ARCHITECTURE, ARE PLACED ALONG DRIVE ISLES. BOLLARD LIGHTING IS PLACED ALONG PEDESTRIAN ROUTES AND IN LANDSCAPE AREAS THAT ARE LOCATED IN OPEN SPACES. STREET LIGHTS WILL BE INSTALLED ALONG GABLE ROAD, PER THE CODE REQUIREMENTS OF THE CITY OF ST. HELENS, OREGON.

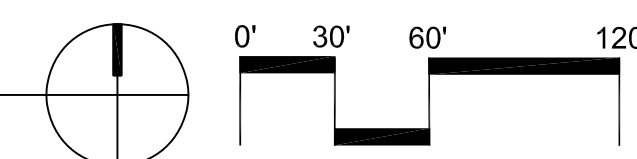
LIGHTING WILL NOT BE INSTALLED ALONG THE EARTHEN TRAIL WITHIN THE UPLAND WETLAND SETBACK AS IT IS AN ENVIRONMENTALLY SENSITIVE AREA.

### SITE MATERIAL LEGEND

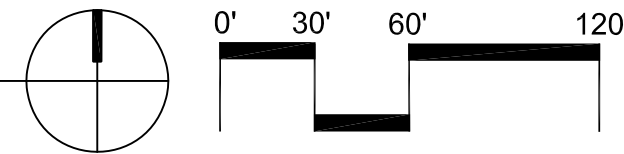
	CONCRETE WALKWAYS
	CONCRETE PAVERS
	GRAVEL PAVING
	LAWN
	PERIMETER SCREENING
	PARKING LOT PLANTING
	PB
	RESTORATION OF WETLAND SETBACK
	PLAYGROUND
	SOUND BARRIER/WALL
	6' HIGH FENCE
	BIKE RACKS - QTY. - 23
	RAISED PLANTER BOXES IN COMMUNITY GARDEN
	POLE LIGHTS AT PARKING LOT / BOLLARD LIGHTS ALONG PEDESTRIAN PATHWAYS IN OPEN SPACES.
	PROPERTY LINE
	UPLAND WETLAND SETBACK
	PROTECTED WETLAND EXTENTS
	PROPOSED TREES
	EXISTING TREES

### LANDSCAPE NOTES



- IRRIGATION WILL BE PROVIDED FOR ALL AREAS OF THE SITE WITH EXCEPTION OF THE WETLAND SETBACK AREA THAT WILL BE PLANTED WITH NATIVE PLANTS.
- FOR PARKING LOT AREAS, DRIVE ISLES, STORMWATER TREATMENT AND GRADING, SEE CIVIL PLANS.
- FOR BUILDING AND OVERALL SITE INFORMATION, SEE ARCHITECTS PLANS.
- SMALL STREET TREES ARE PROVIDED ALONG GABLE ROAD AND ARE SPACED AT 20' O.C.. THE TREE SIZE AT FULL MATURITY WILL BE NO MORE THAN 25' TALL AND 16' WIDE.
- TREES ARE NOT PLANTED CLOSER THAN 20' FROM AN INTERSECTION.
- BIKE RACKS ARE LOCATED WITHIN THE BUILDINGS AND DISPERSED THROUGHOUT THE SITE IN CLOSE PROXIMITY TO EACH BUILDING.
- A 6' HIGH SITE OBSCURING FENCE WILL BE INSTALLED ALONG THE WEST PROPERTY LINE TO SERVE AS A VISUAL SCREEN FROM NEIGHBORING PROPERTIES. A 10' MINIMUM VEGETATIVE BUFFER IS PROVIDED ALONG THE WEST PROPERTY LINE WHERE INDICATED. SMALL TO MEDIUM SIZED TREES, SHRUBS AND GROUND COVER WILL BE PLANTED ACCORDING TO 17.72.070.4-5.
- PARKING LOT ISLANDS WILL BE PLANTED WITH TREES, SHRUBS AND GROUND COVER TO PROVIDE SCREENING WITHIN PARKING LOTS PER CODE REQUIREMENTS.
- SITE TRIANGLES ARE SHOWN AT 35' ALONG GABLE ROAD. TREES ARE LOCATED OUTSIDE OF THE SITE TRIANGLE.
- INVASIVES WILL BE REMOVED AND NATIVE PLANTINGS WILL BE INSTALLED WITHIN THE UPLAND SETBACK.







LEGEND

	PRIVATE OUTDOOR AREAS (BY MWA)
	SHARED OUTDOOR AREAS

PRIVATE OUTDOOR AREA CALCULATIONS

PRIVATE OUTDOOR AREA REQUIREMENTS	
NUMBER OF UNITS	238
PRIVATE OUTDOOR SIZE PER UNIT	48 S.F.
TOTAL PRIVATE OUTDOOR SPACE REQUIRED	11,424 S.F.
PRIVATE OUTDOOR AREA PROVIDED	
NUMBER OF UNITS	238
PRIVATE OUTDOOR SIZE:	
FAMILY HOUSING UNIT (1BR/2BR/3BR)	
FIRST FLOOR	57 SF/UNIT X 42 UNITS = 2,394 S.F. S.F.
SECOND AND THIRD FLOOR	102 SF/UNIT X 84 UNITS = 8,568 S.F. S.F.
TOWNHOUSE UNIT	80 SF/UNIT X 31 UNITS = 2,480 S.F. S.F.
TOTAL PRIVATE OUTDOOR SPACE PROVIDED	13,442 S.F.

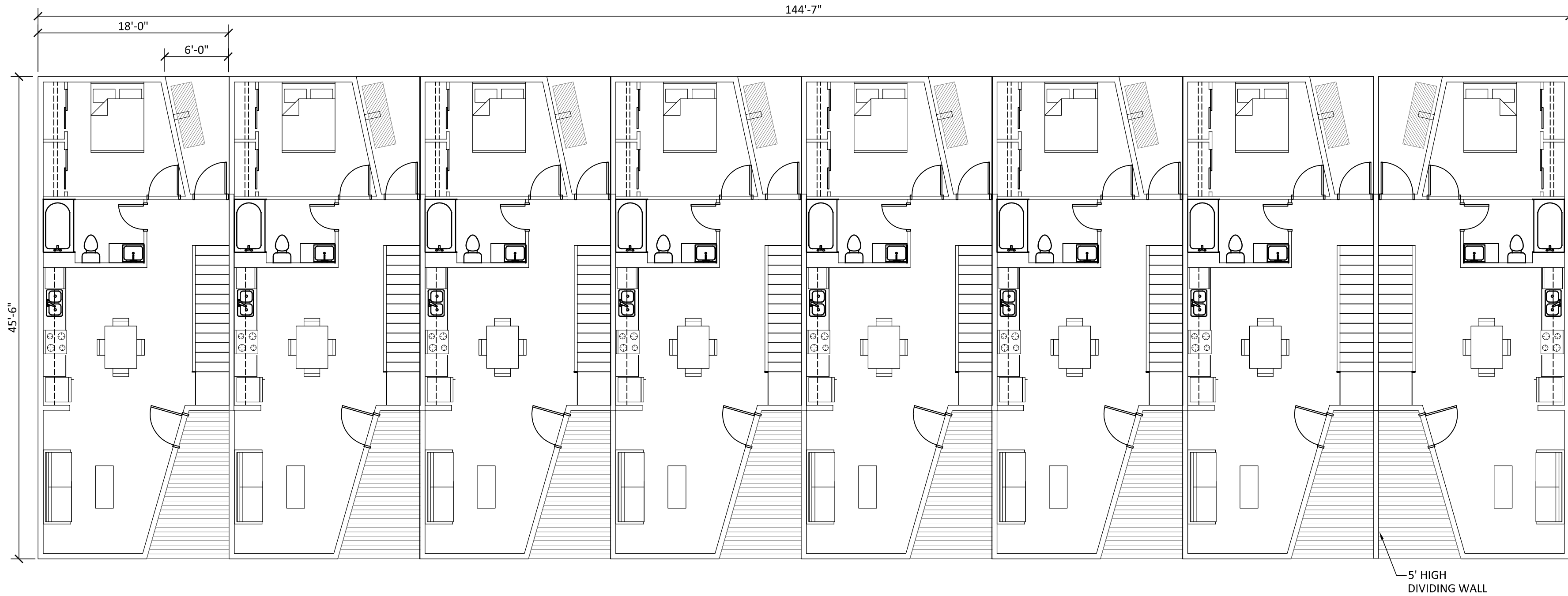
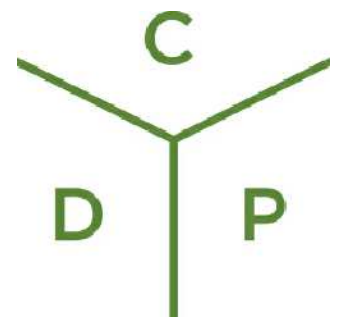
SHARED OUTDOOR AREA CALCULATIONS

TOTAL PRIVATE OUTDOOR SPACE REQUIRED	51,300 S.F.
SHARED OUTDOOR AREA PROVIDED	
SHARED AREA ADJACENT TO BUILDINGS (1-4)	17,841 S.F.
SHARED AREA ADJACENT TO BUILDINGS (5)	19,555 S.F.
SHARED AREA ADJACENT TO BUILDINGS (6-11)	58,610 S.F.
WETLAND TRAIL	11,065 S.F.
TOTAL PRIVATE OUTDOOR SPACE PROVIDED	107,071 S.F.

SHARED OUTDOOR AREA NOTES

1. PLAYGROUNDS, COMMUNITY COURTYARDS AND COMMUNITY GARDENS ARE INCLUDED IN SHARED OUTDOOR AREA CALCULATIONS.
2. ALL LAWN AREAS THROUGHOUT THE SITE ARE INCLUDED IN THE SHARED OUTDOOR AREA CALCULATIONS.
3. ALL WALKWAYS THAT ARE ADJACENT TO AND INSIDE SHARED OUTDOOR AREAS ARE COUNTED IN THE CALCULATIONS.

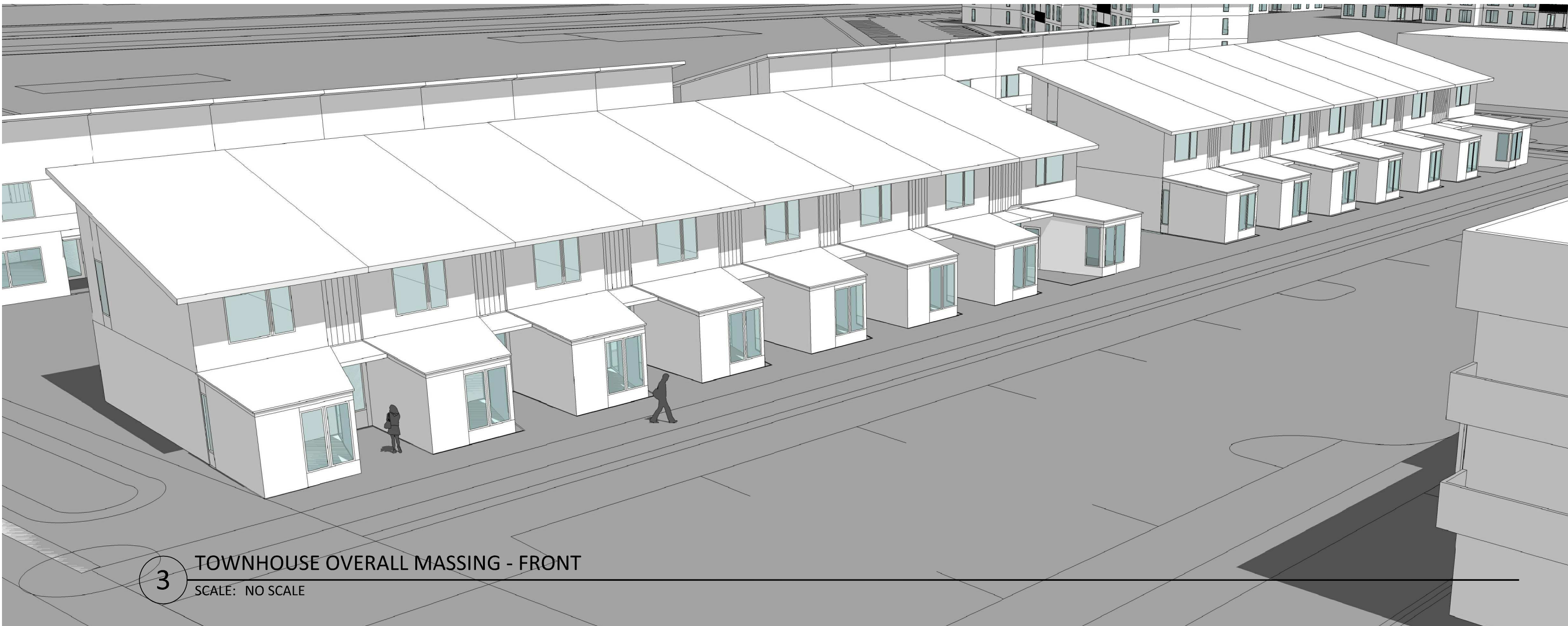




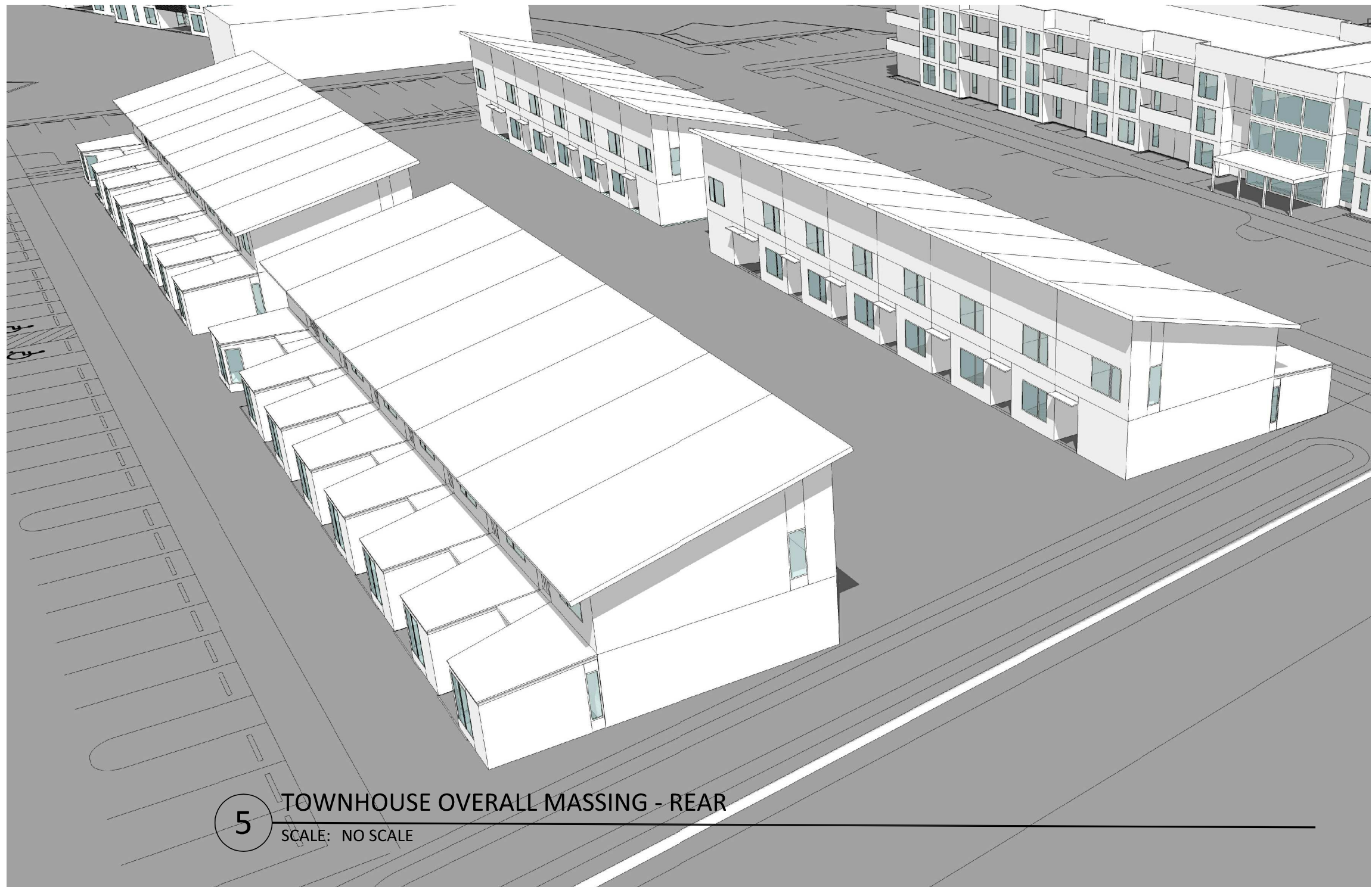
1 TOWNHOUSE BUILDING PLAN  
SCALE: 1/8" = 1'-0"



2 3 BEDROOM TOWNHOUSE UNIT PLANS  
SCALE: 1/8" = 1'-0" 1250 SF



3 TOWNHOUSE OVERALL MASSING - FRONT  
SCALE: NO SCALE



5 TOWNHOUSE OVERALL MASSING - REAR  
SCALE: NO SCALE



4 TOWNHOUSE STREET ELEVATION  
SCALE: NO SCALE

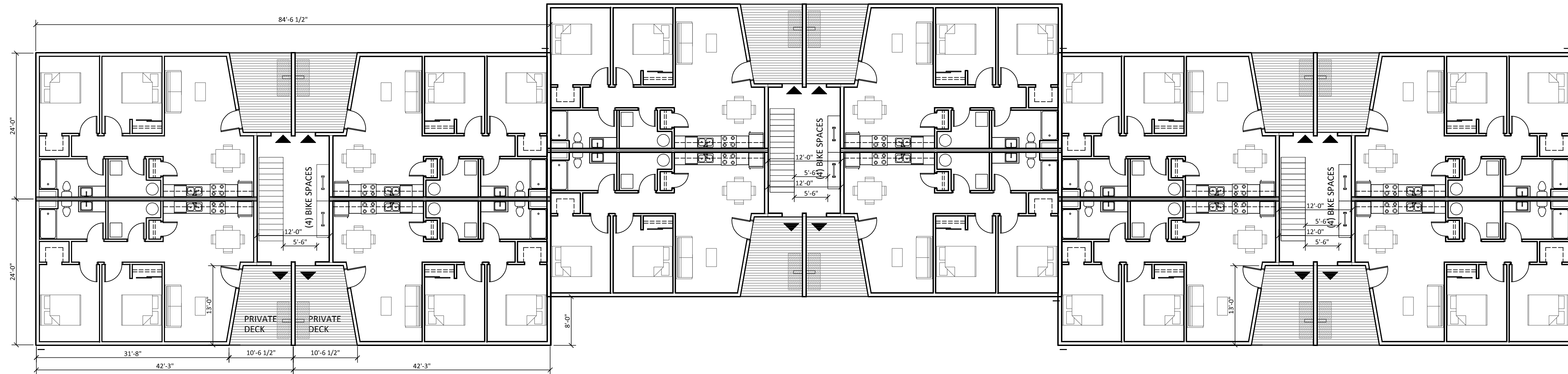
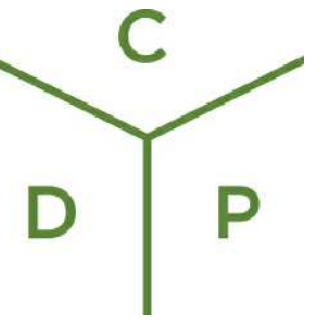
REVISION NO	DATE

COMMUNITY  
DEVELOPMENT  
PARTNERS  
ST. HELENS  
HOUSING

GABLE ROAD  
ST. HELENS, OR 97051  
ISSUANCE  
CONDITIONAL USE  
DATE  
08/14/2019  
SCALE

PROJECT NUMBER  
2019103.00  
DRAWN BY  
DM  
DRAWING TITLE  
TOWNHOUSE  
BUILDING  
PLANS / ELEV.  
SHEET NUMBER





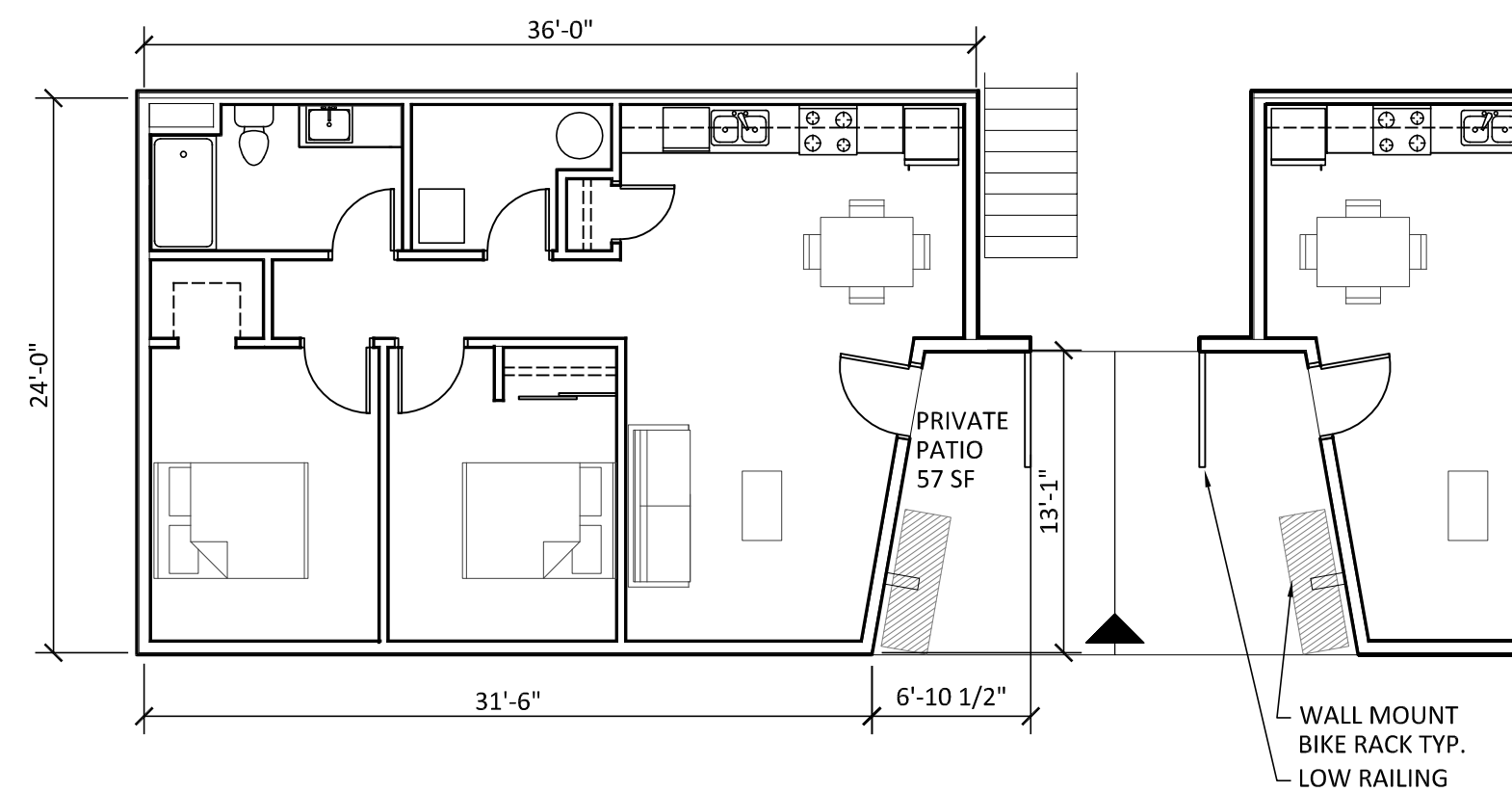
**1** TYPICAL FAMILY HOUSING BUILDING PLAN (STACKED FLATS - ALL FLOORS THE SAME PLAN)  
SCALE: 3/32" = 1'-0"



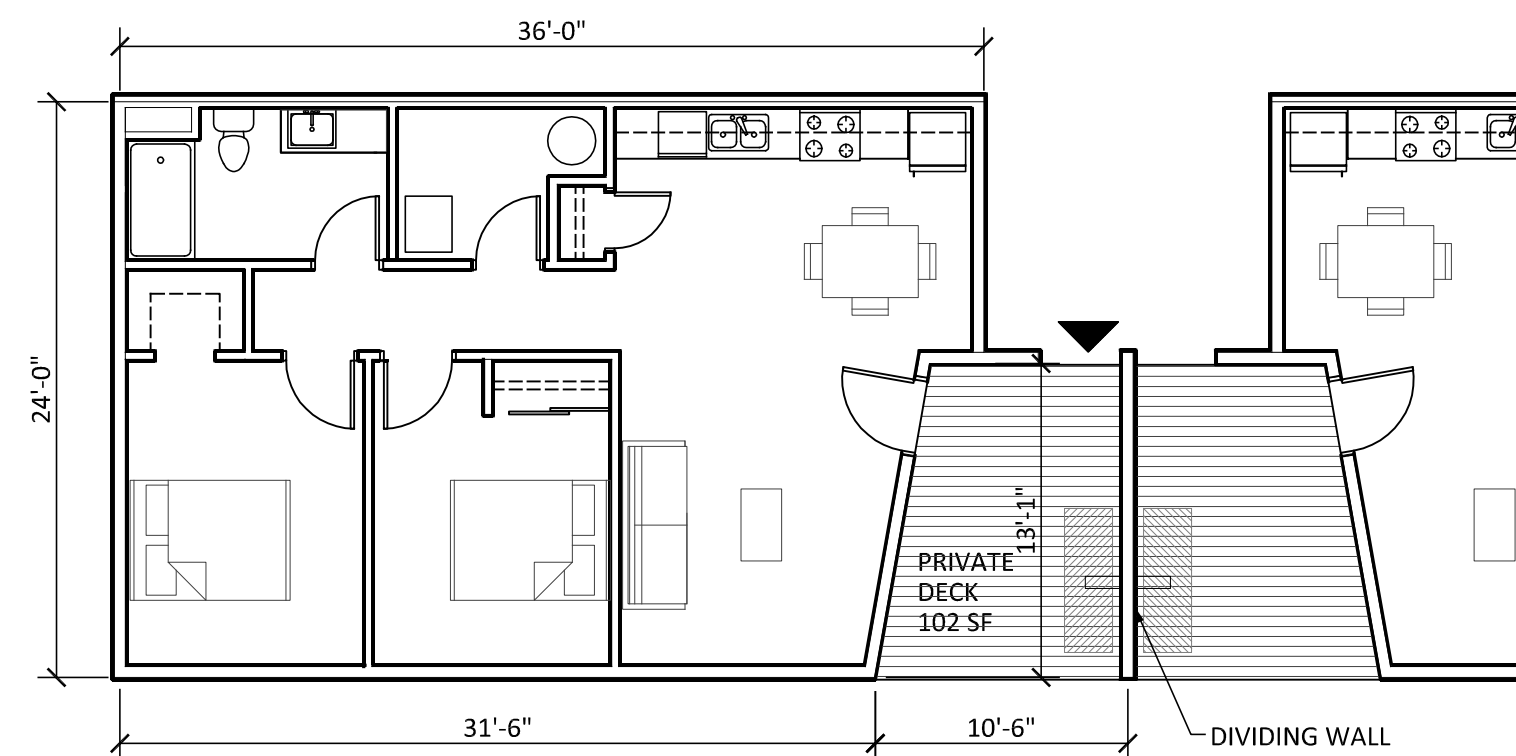
2 FAMILY HOUSING OVERALL MASSING  
SCALE: NO SCALE



3 FAMILY HOUSING ENLARGED ELEVATION  
SCALE: NO SCALE



4 TWO BEDROOM FLAT GROUND FLOOR PLAN (1 AND 3 BR SIM)  
SCALE: 1/8" = 1'-0" 819 SF



5 TWO BEDROOM FLAT UPPER FLOOR PLAN (1 AND 3 BR SIM)  
SCALE: 1/8" = 1'-0" 819 SF

[illegible]

COMMUNITY  
DEVELOPMENT  
PARTNERS  
T. HELENS  
HOUSING

ABLE ROAD  
T. HELENS, OR 97051

SUANCE  
CONDITIONAL USE

DATE  
3/14/2019

## SCALE

PROJECT NUMBER  
019103.00

DRAWN BY  
M

DRAWING TITLE  
FAMILY

## FAMILY HOUSING

PLANS / ELEVS.

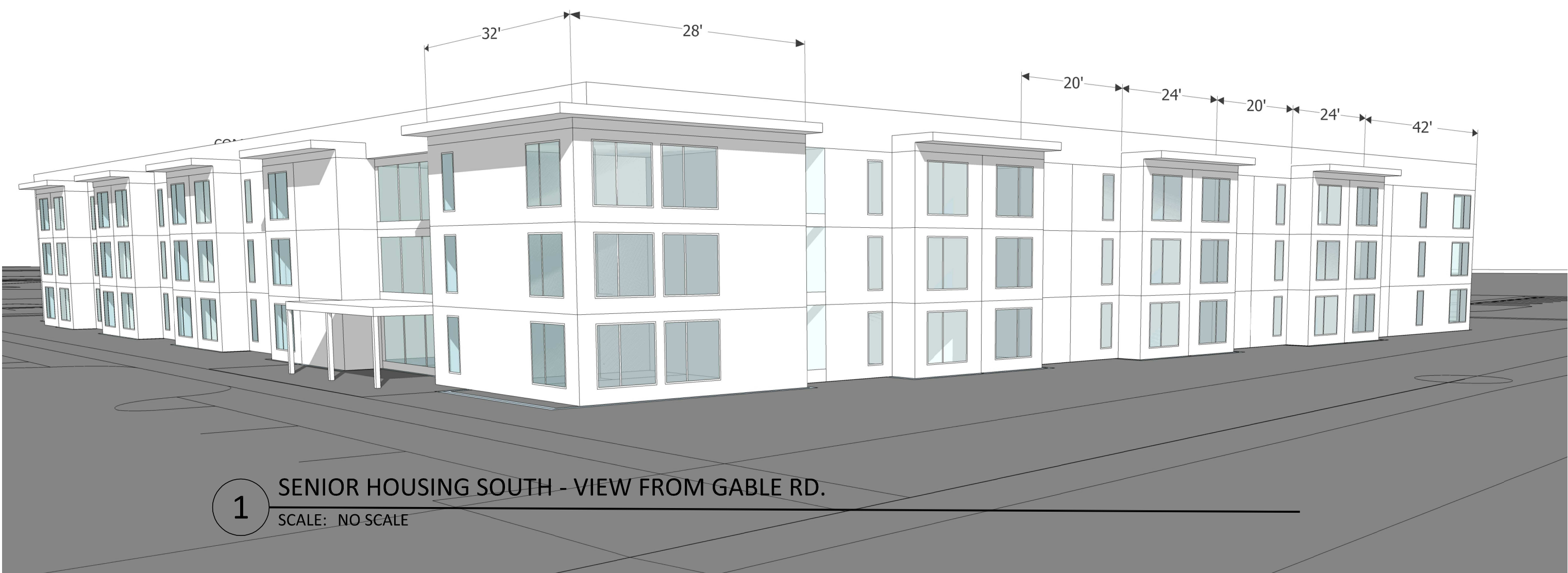
SHEET NUMBER

## A2.1





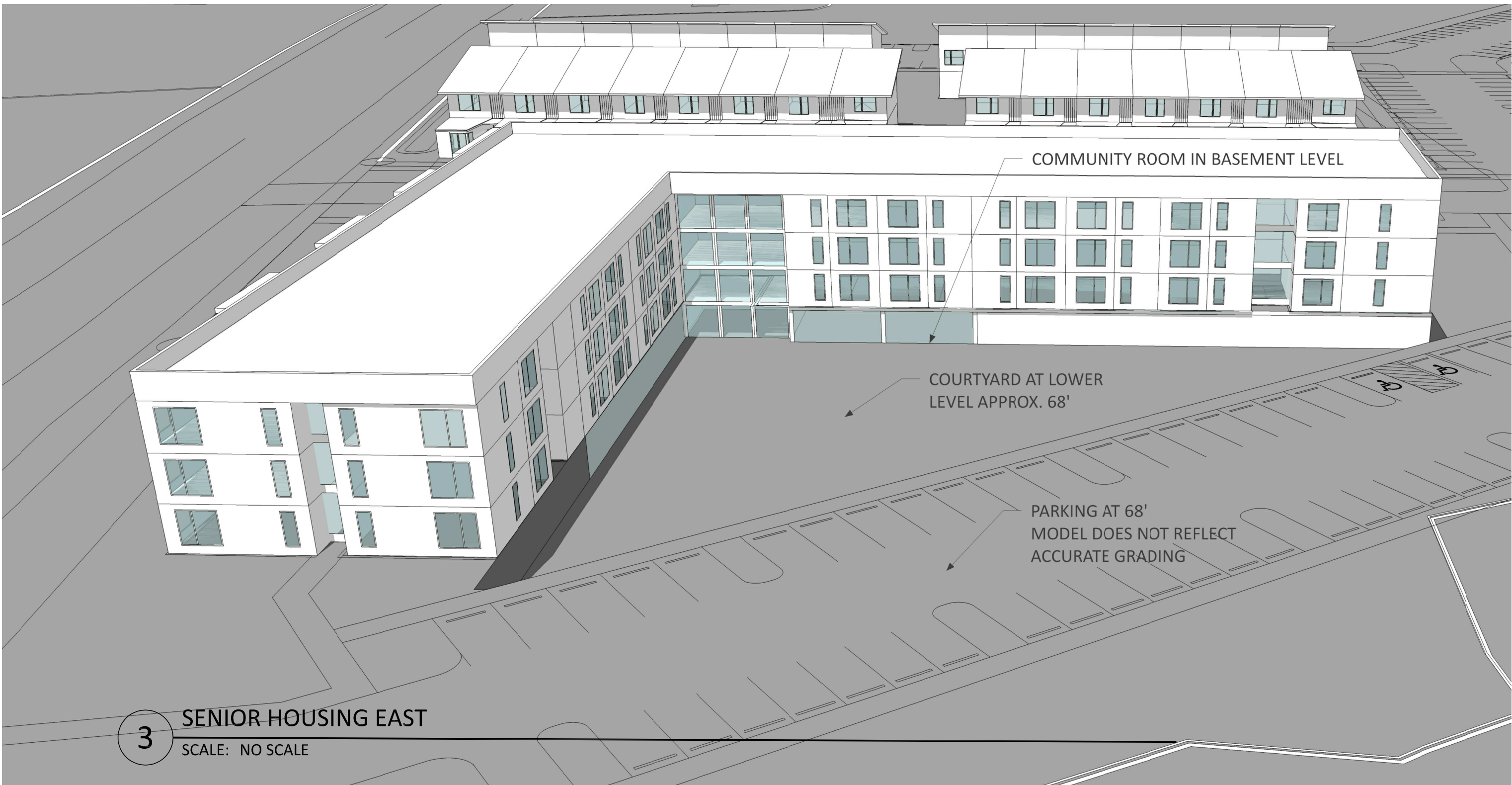




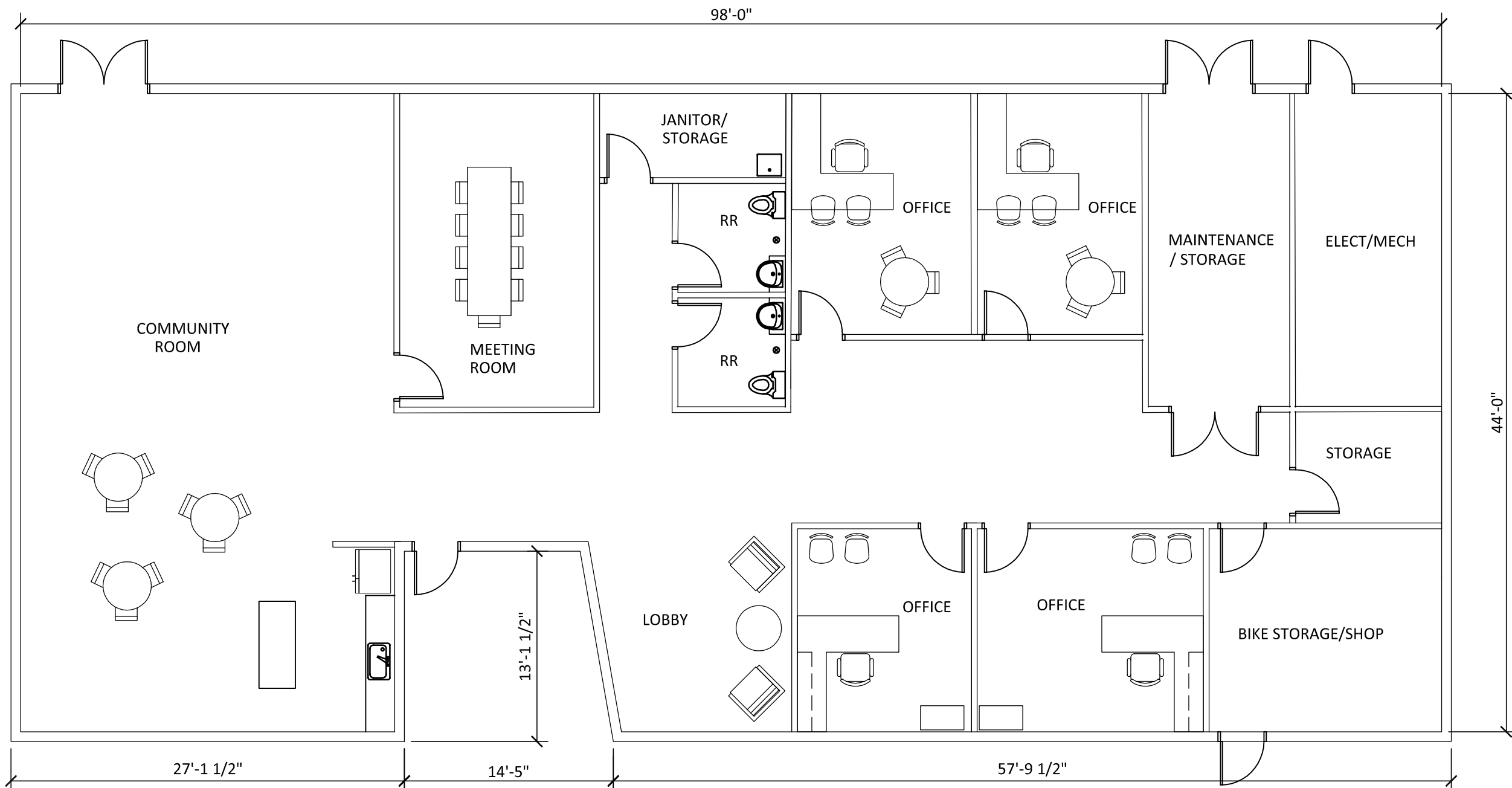
1 SENIOR HOUSING SOUTH - VIEW FROM GABLE RD.  
SCALE: NO SCALE



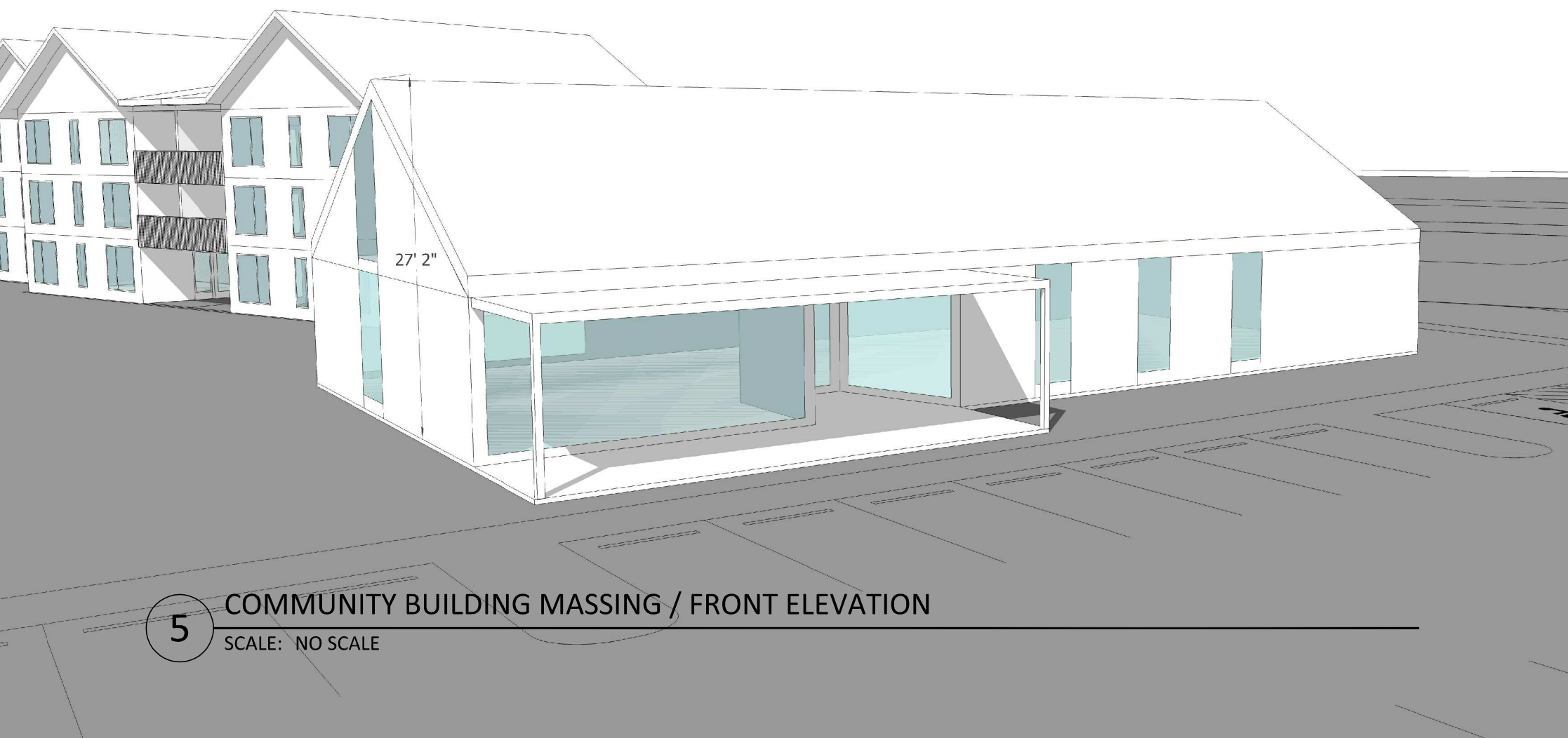
2 SENIOR HOUSING WEST (VIEW FROM DRIVEWAY)  
SCALE: NO SCALE



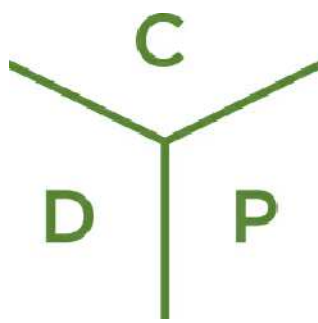
3 SENIOR HOUSING EAST  
SCALE: NO SCALE



4 COMMUNITY BUILDING FLOOR PLAN  
SCALE: 1/8" = 1'-0" 4330 SF



5 COMMUNITY BUILDING MASSING / FRONT ELEVATION  
SCALE: NO SCALE



REVISION NO	DATE

COMMUNITY  
DEVELOPMENT  
PARTNERS  
ST. HELENS  
HOUSING

GABLE ROAD  
ST. HELENS, OR 97051

ISSUANCE  
CONDITIONAL USE

DATE  
08/14/2019

SCALE

PROJECT NUMBER  
2019103.00

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DM

DRAWING TITLE  
**SENIOR  
HOUSING /  
COMMUNITY**  
SHEET NUMBER





mwa architects

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AUG 14 2019  
CITY OF ST. HELENS

SAN FRANCISCO OAKLAND PORTLAND

## Memorandum

To: Jacob A Graichen, City Planner, City of St. Helens

From: Diana Moosman

Project/No: 201913.00 CDP St. Helens Apartments

This memo is to accompany our Completeness Response letter dated 8/14/19 in response to specific items noted by the City of St. Helens. Our approach to the following items is described below:

1) *"We will need to ask the Planning Commission about the 39' building height of the Family Housing building. It's possible they will require a variance. This isn't a deficiency in the application, rather a notice of potential additional permitting and a matter I believe we have already discussed"*

Chapter 17.68.010 allows for projections into the height limitation for objects not used for human occupancy. We would argue that the additional 5' of height within the attic is not used for human occupancy.

The design intent is for a steeper roof pitch to create more vertically proportioned massing. We would be limited to a 3:12 roof pitch to adhere to a 35' maximum building height.

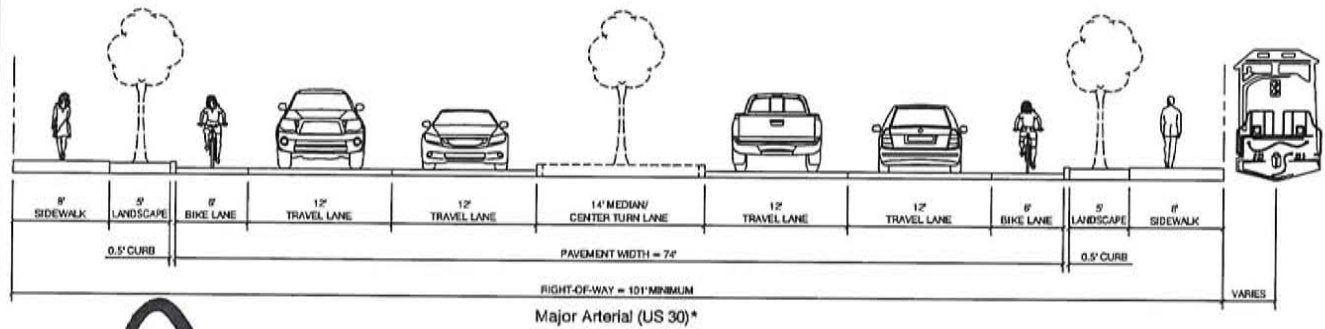
2) *"Lighting type is a bit vague. It's clear that pole and bollard lighting is proposed throughout. The lighting narrative on Sheet L1.01 was also observed. Is there any more detail for the proposed lighting?"*

In order to specify light fixtures we need to complete a full site photometric plan. We intend to meet all city requirements for site lighting and will provide a photometric plan with our permit application. Can this be noted in the staff report as a condition of approval rather than an item for application completeness?

3) *"If the intent is to use SHMC 17.108.080 (3) (c) for outdoor space requirements areas need to be quantified to prove"*

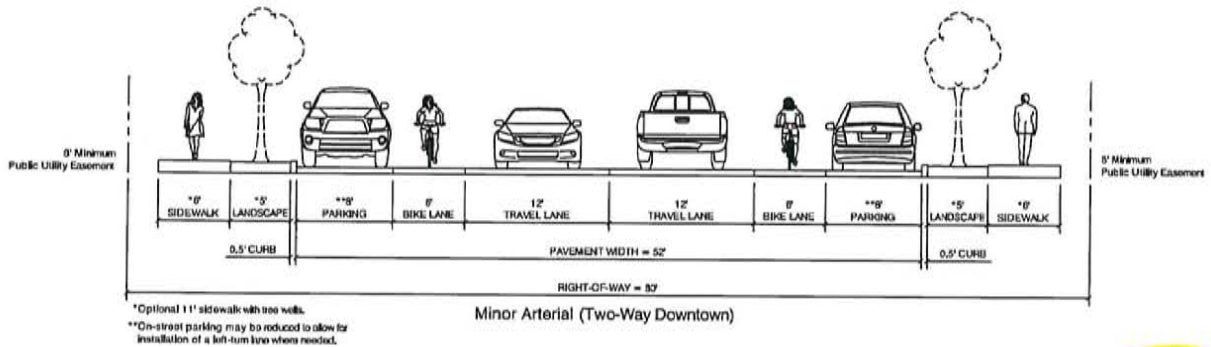
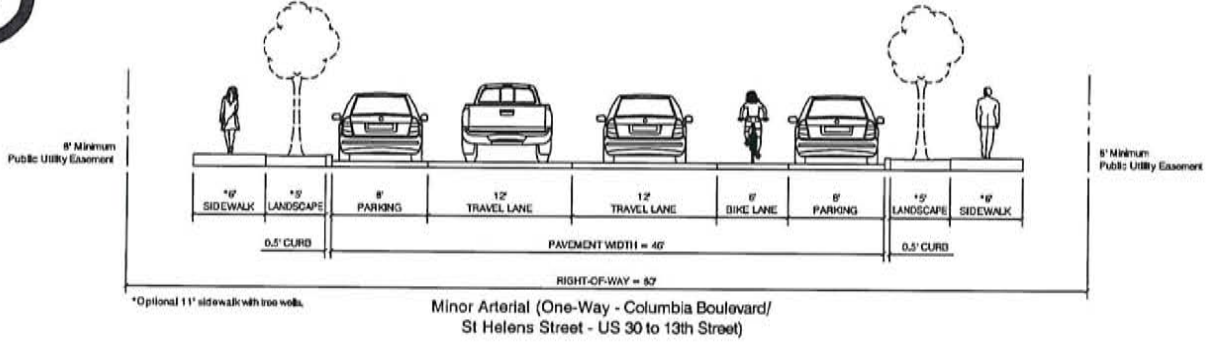
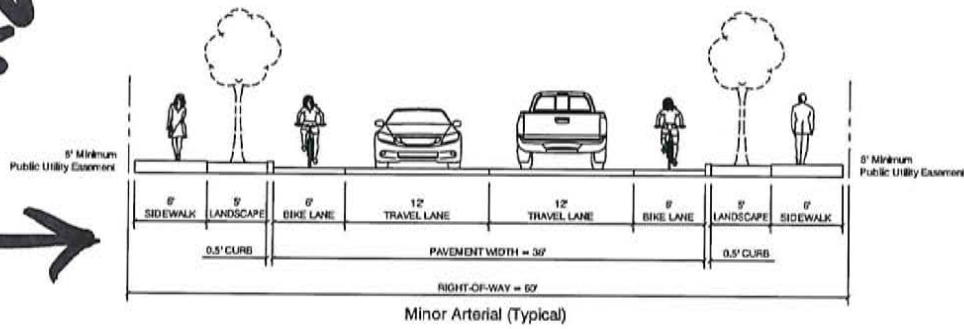
We have created a new exhibit quantifying all outdoor areas – see L1.02. We exceed the requirement for shared outdoor space with this site plan and have removed all private outdoor areas from the Senior Housing Building.





**GABLE RD.**

↑



**LEGEND**



TREES TO BE PROVIDED AS APPROPRIATE PER CITY CODE AND LOCATION SPECIFIC CONSIDERATIONS

\*The US 30 Cross-Sections are shown to be consistent with ODOT Standards. Specific roadway designs will be developed through a refinement plan or project development process. Design and future improvements to US 30 must also address ORS 366.215 (Reduction in Vehicle Carrying Capacity) on this national freight network facility.



**STANDARD CROSS SECTIONS  
ST. HELENS, OREGON**

FIGURE  
**7-2**



## Recommended Street Section

A new preferred option was identified through further discussion and review with members of the advisory committee, Planning Commission, City Council and other community members. It includes one-way cycletracks on both sides of the street.

Each cycletrack is separated from the roadway by a landscaping strip, and each will be separated from the adjacent pedestrian walkway by a curb or other means. The new option has the same safety benefits of the original Option C but can be transitioned to adjacent segments more effectively.

Implementation of this option will require careful planning to allow for safe, convenient transitions between this design and parts of the roadway that have undergone recent improvements. Typical transitions are illustrated in the conceptual intersection designs in this document. The final design will also need to ensure that intersections and access points address bicycle and pedestrian safety and minimize conflicts between bicyclists, pedestrians and motor vehicles. Examples of treatments for similar facilities are found in Appendix 8.

This cross-section also includes a center lane to accommodate turn lanes needed at key intersections or other access points to maintain traffic mobility. A landscaped median could be used in short portions of this segment but will need to be located to continue to ensure access to local businesses in this part of the corridor.



## SEGMENT 4.2: GABLE ROAD





851 SW 6th AVENUE, SUITE 600  
 PORTLAND, OR 97204  
 P 503.228.5230 F 503.273.8169

## MEMORANDUM

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Date:	June 27, 2019	Project #: 24071.0
To:	Jacob Graichen, City of St Helens Ken Shonkwiler, Oregon Department of Transportation	
From:	Brian Dunn, P.E., Matt Bell, Kittelson & Associates, Inc.	
Project:	St Helens Housing Development	
Subject:	Traffic Impact Analysis	

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## INTRODUCTION

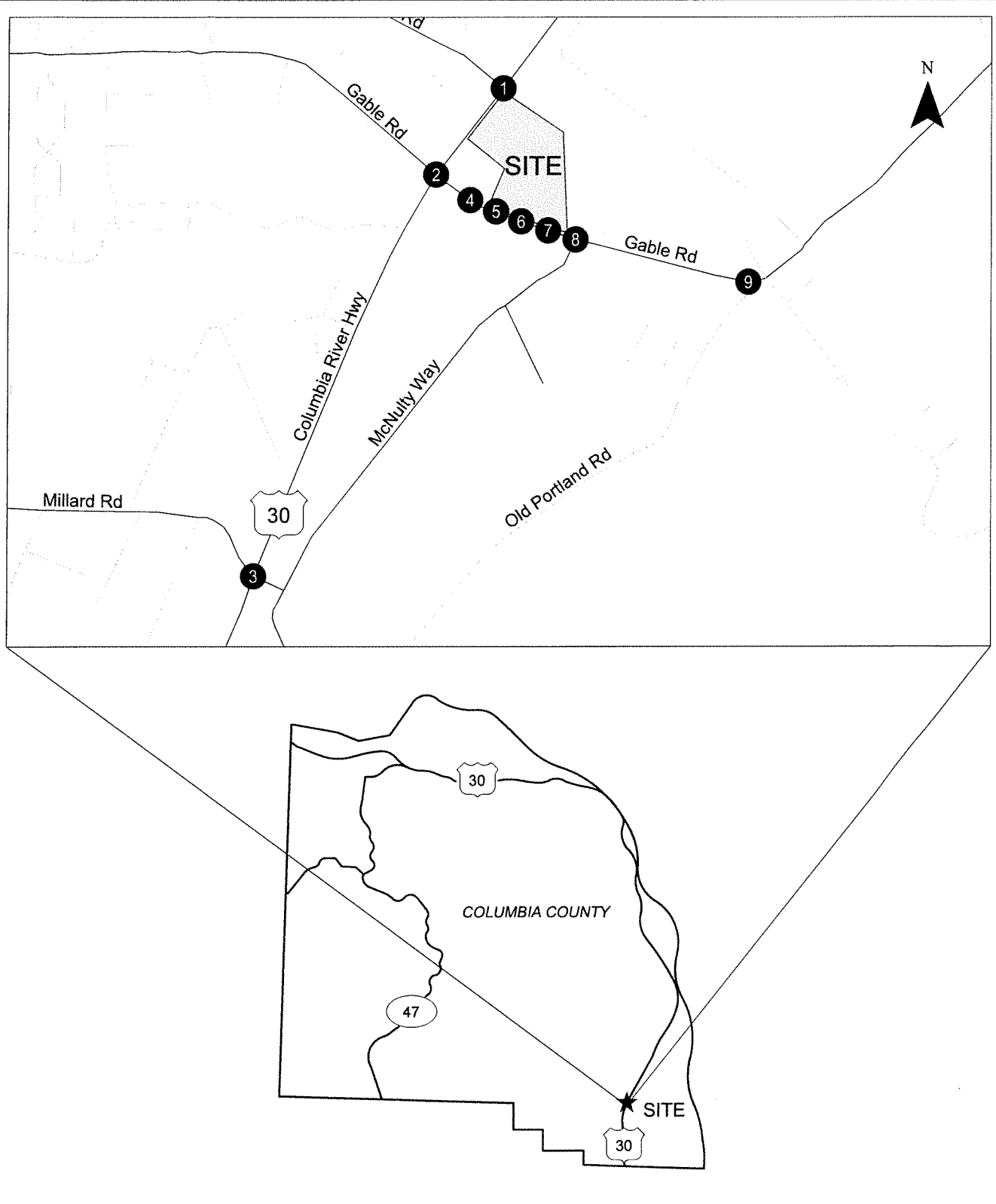
Community Development Partners is proposing to develop the 14.42-acre site located on the north side of Gable Road and the east side of US 30 in St Helens. Figure 1 illustrates the site vicinity map. The proposed development plan includes construction of 238 apartment units along with a clubhouse, playground, and other amenities, including 437 parking spaces. Access to the proposed development is planned to be provided via two new driveways located along the north side of Gable Road, approximately 660 feet and 990 feet east of US 30. One additional driveway is planned to be provided to the shared access road to the west of the site frontage for emergencies pending an agreement with adjacent property owners. Figure 2 illustrates the conceptual site plan. Construction of the St Helens Housing development is expected to occur in 2019 with full build-out and occupancy in 2021.

The results of this study indicate that the proposed St Helens Housing development can be constructed while maintaining acceptable traffic operations and safety at the study intersections. No off-site transportation mitigation needs were identified. Landscaping, above ground utilities, and signing should be located and maintained along the site frontage and throughout the site in a manner that preserves adequate intersection sight distance.

## SCOPE OF THE REPORT

This analysis determines the transportation-related impacts associated with the proposed St Helens Housing Development and was prepared in accordance with City of St. Helens and Oregon Department of Transportation (ODOT) requirements for a traffic impact analysis. The study intersections included in the analysis were selected based on a review of Section 17.156.050 of the St Helens Municipal Code and discussions with City and ODOT staff. The study intersections include all major intersections where site-generated traffic is expected to exceed 20 vehicles during the peak hour along with all site-access points and intersections adjacent to the proposed development site. *Appendix "A" contains the scoping memo, which was reviewed by the City and ODOT.*





## - Study Intersection

Site Vicinity Map  
St Helens, Oregon

Figure  
1

H:\21\24071 - St Helens Affordable Housing\report\figs\24071\_Fig1.dwg Jun 25, 2019 - 2:13pm - arazmpa Layout Tab: Fig01







Based on scoping direction, the study intersections include:

1. Columbia River Highway (US 30)/Sykes Road
2. US 30/Gable Road
3. US 30/Millard Road
4. W Walmart Driveway-Shared Access Road/Gable Road
5. E Walmart Driveway/Gable Road
6. New Access Driveway 1/Gable Road
7. New Access Driveway 2/Gable Road
8. McNulty Way/Gable Road
9. Old Portland Road/Gable Road

This report evaluates these transportation issues:

- Year 2019 existing land-use and transportation-system conditions at the study intersections during the weekday AM and PM peak hours;
- Approved but not yet constructed developments and transportation improvements planned in the study area;
- Year 2021 background traffic conditions (without the proposed development) at the study intersections during the weekday AM and PM peak hours;
- Trip generation and distribution estimates for the proposed development;
- Year 2021 total traffic conditions (with full build-out and occupancy of the proposed development) during the weekday AM and PM peak hours; and
- On-site driveway operations.

#### Performance Measures and Mobility Targets

Traffic operations at the study intersection and site access driveways were evaluated based on the applicable performance measures identified in the St Helens Transportation System Plan (TSP – Reference 1). Per the TSP, the City requires all signalized intersections to operate at LOS “D” or better and maintain a volume-to-capacity (v/c) ratio at or below 1.0. For two-way stop-controlled intersections, LOS “E” is acceptable for the worst approach and LOS “F” when a traffic signal is not warranted.

Traffic operations at the study intersections along US 30 were evaluated based on the applicable mobility targets identified in Table 6 of the Oregon Highway Plan (OHP – Reference 2). The OHP identifies mobility targets for all signalized and unsignalized intersections along US 30 as well as policies that provide guidance on the application of the mobility targets. Per Table 6 of the OHP, the US 30/Sykes Road and US 30/Gable Road intersections have a mobility target of 0.85 while the US 30/Millard Road intersection has a mobility target of 0.80 for the state approaches and 0.90 for the non-state approaches. When the US 30/Millard Road intersection is signalized, it will have a mobility target of 0.80.



## Analysis Tools and Methodology

All analyses described in this report were performed in accordance with the procedures stated in the *Highway Capacity Manual, 6<sup>th</sup> Edition* (HCM – Reference 3). Synchro 10 was used to conduct the analysis. Synchro 10 is a software tool designed to assist with operations analysis in accordance with HCM methodologies. The critical intersection analysis results for signalized intersections were hand-calculated based on Synchro 10 generated reports consistent with the ODOT Analysis Procedures Manual (APM – Reference 4).

All analyses used the peak 15-minute flow rates that occurred during the weekday morning and evening peak hours. Using the peak 15-minute flow rates ensures that this analysis is based on a reasonable worst-case scenario.

## EXISTING CONDITIONS

The existing conditions analysis identifies the site conditions and current physical and operational characteristics of key roadways within the study area. These conditions will be compared with future conditions later in this report.

## SITE CONDITIONS AND ADJACENT LAND USES

The proposed development site is located within the St. Helens city limits, is currently vacant, and is zoned for General Commercial (GC) development. Adjacent land uses include additional GC to the north, east, and south along Gable Road and Highway Commercial (HC) west of US 30, per the *City of St. Helens Zoning Map* (Reference 5).

## TRANSPORTATION FACILITIES

Table 1 summarizes the characteristics of roadways within the site vicinity.

**Table 1: Existing Transportation Facilities**

Roadway	Functional Classification <sup>1</sup>	Number of Lanes	Posted Speed (mph)	Sidewalks	Bicycle Lanes	On-Street Parking
US 30	Major Arterial / Statewide Freight Route <sup>2</sup>	4-5 Lanes	35/45 <sup>3</sup>	West Side	Yes	No
Sykes Road	Collector	2 Lanes	25	Partial <sup>4</sup>	No	No
Gable Road	Minor Arterial/Collector <sup>5</sup>	2 Lanes	25	Partial <sup>6</sup>	Yes <sup>7</sup>	No
Millard Road	Minor Arterial	2 Lanes	40	No	No	No
McNulty Way	Collector Street	2 Lanes	25	Partial <sup>8</sup>	Partial <sup>9</sup>	No
Old Portland Road	Minor Arterial	2 Lanes	40	Partial <sup>10</sup>	Yes <sup>11</sup>	No

1. Per *City of St. Helens Transportation System Plan* (TSP – Reference 1).

2. Per *Oregon Highway Plan* (OHP-Reference 2)

3. Speed limit on US 30 is 35 miles per hour near Gable Road and 45 mph near Millard Road.

4. The southern sidewalk terminates at Bowling Alley Lane.

5. Gable Road is classified as a minor arterial east of US 30 and a collector west of US 30.

6. Sidewalks are provided on both sides of the roadway approaching US 30.

7. Bicycle Lanes are provided on both sides of the roadway east of US 30.



8. Sidewalks are provided on both sides of the roadway adjacent to recent development.
9. Bike lanes are provided on both sides of the roadway from Gable Road to the south city limits.
10. Sidewalks are provided on the west side of the roadway adjacent to McCormick Park.
11. Bike lanes are provided on both sides of the roadway north of Gable Road.

### Roadway Facilities

As shown in Figure 2, Gable Road is the only city street located adjacent to the proposed development site. Gable Road has a three-lane cross section adjacent to the site, with dedicated left-turn lanes into commercial developments to the north and south and to a former access to the site. Gable Road connects the site to schools, parks, churches, and retail uses on the east and west sides of US 30 and to downtown St Helens via Old Portland Road. Figure 3 illustrates the existing lane configurations and traffic control devices at the study intersections.

### Pedestrian and Bicycle Facilities

Sidewalks are provided on both sides of Gable Road to the west of the proposed development site. Sidewalks are not provided adjacent to the site or to the east. The closest marked crosswalks are provided at the US 30/ Gable Road intersection. Bike lanes are provided on both sides of Gable Road adjacent to the site as well as further to the east and west.

### Transit Facilities

Columbia County Rider (CCR) provides transit service within the site vicinity. Route 3, the South County Flex, operates Monday through Friday from 7:30 AM to 4:00 PM on approximately 90-minute headways. Route 3 connects St. Helens to Scappoose with stops at the Walmart and CCMH Creekside on Gable Road. The county also offers dial-a-ride service for seniors, individuals with disabilities, and citizens. Dial-A-Ride is available in the site vicinity Monday through Friday from 7:30 AM to 7:00 PM. CCR does not provide service on weekends or on federal holidays.

## TRAFFIC VOLUMES AND PEAK HOUR OPERATIONS

Manual turning movement counts were conducted at the study intersections in June 2019 when school was in-session and no inclement weather was present that affected typical traffic patterns.<sup>1</sup> The counts were conducted on a typical mid-week day during the morning (7:00 to 9:00 AM) and evening (4:00 to 6:00 PM) peak time periods. The system-wide morning and evening peak hours were found to occur from 7:40 to 8:40 AM and 4:40 to 5:40 PM, respectively. Figure 4 summarizes the turning movement counts for the weekday morning and evening peak hours. *Appendix "B" contains the traffic count worksheets used in this study.*

---

<sup>1</sup> The counts were conducted on Tuesday, June 11, 2019, which was a typical school day for all grade levels except seniors who graduated Friday, June 7, 2019. The counts were compared to historical counts conducted in January and May 2018 and found to be generally consistent, particularly total entering volumes (TEV). In some instances, the TEV at the US 30/Gable Road intersection were higher in June 2019, than in January and/or May 2018.



The volumes along US 30 were seasonally adjusted to 30<sup>th</sup> Highest Hour Design Volumes (30 HV) in accordance with the Seasonal Trend Table methodology identified in the APM. A combination of the Commuter and Summer seasonal trends were used as a basis for the adjustment consistent with the St Helens TSP and subsequent planning efforts. All other traffic on City streets was not adjusted.

#### Current Levels of Service

Figure 4 summarizes the results of the existing traffic conditions analysis. As shown, all the study intersections currently operate acceptably during the weekday AM and PM peak hours per their respective mobility standards and targets with the exception of the US 30/Gable Road and US 30/Millard Road intersections. *Appendix "C" includes the existing traffic conditions worksheets.*

#### *US 30/Gable Road*

The US 30/Gable Road intersection currently exceeds its applicable mobility target under existing traffic conditions during the weekday PM peak hour. The TSP identifies a long-term (2022 to 2031) improvement at the intersection that includes the addition of a westbound right-turn lane. The City is currently collecting proportionate share contributions from developers for the improvement.

It should be noted that the westbound approach to the US 30/Gable Road intersection is currently striped with a separate left-turn lane and shared through/right-turn lane. However, the width of the shared through/right-turn lane is sufficient to accommodate separate through and right-turn movements for passenger vehicles; therefore, many vehicles can be seen "jumping the queue" to complete a right turn. Despite the observed operations at the intersection, a defacto right-turn lane was not included to ensure a conservative analysis.<sup>2</sup>

#### *US 30/Millard Road*

The US 30/Millard Road intersection currently exceeds its applicable mobility target under existing traffic conditions during the weekday AM and PM peak hours. ODOT is planning to install a traffic signal at the intersection later this year.

#### Traffic Safety

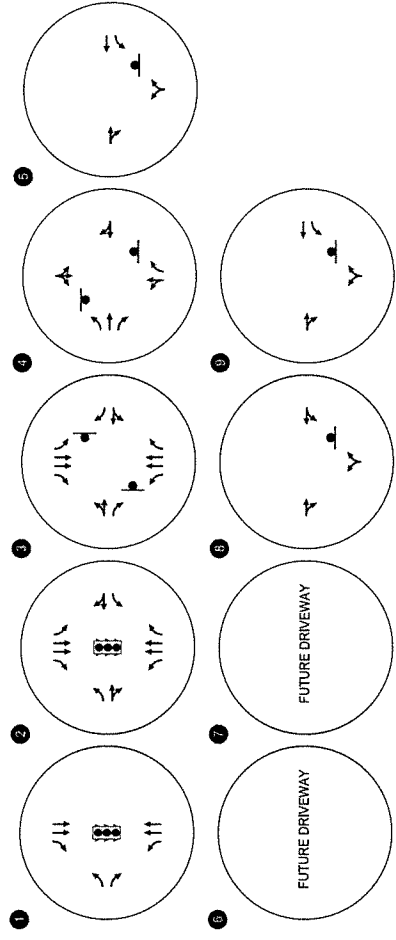
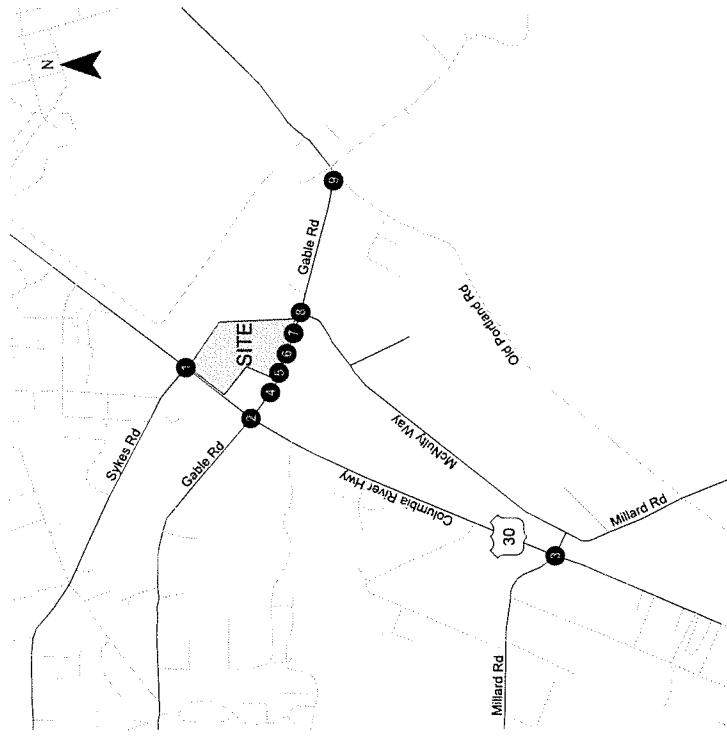
The crash history of the study intersections was reviewed in an effort to identify potential safety issues that could impact access to the site. Crash data was obtained from ODOT for the five-year period from January 1, 2013 through December 31, 2017. Table 2 summarizes the crash data for the study intersections, including the number, type and severity of crashes over the five-year period.

Crash rates per million entering vehicles (MEV) were developed for each study intersection. The crash rates were compared to the 90<sup>th</sup> percentile rates for similar facilities provided in Table 4-1 of the ODOT APM. Any intersection that has a crash rate equal to or greater than the corresponding 90<sup>th</sup> percentile rate is recommended for further review.

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<sup>2</sup> Despite near-term acceptable LOS operations, the City should continue to pursue a separate right-turn lane at the intersection in the future to accommodate anticipated travel demand as identified in the City's TSP.



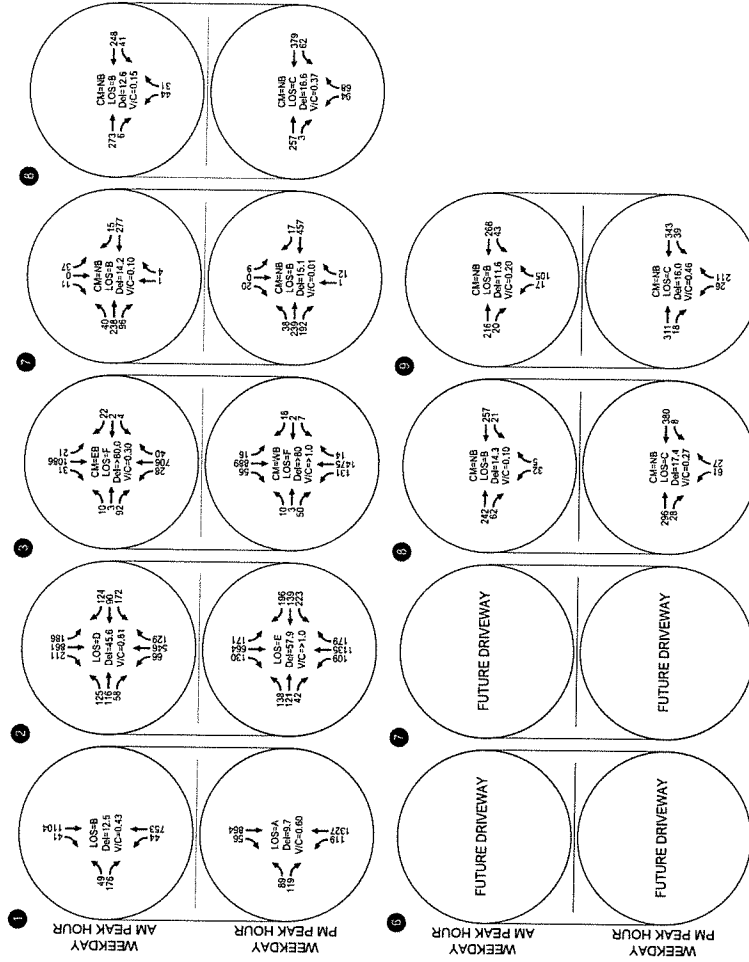
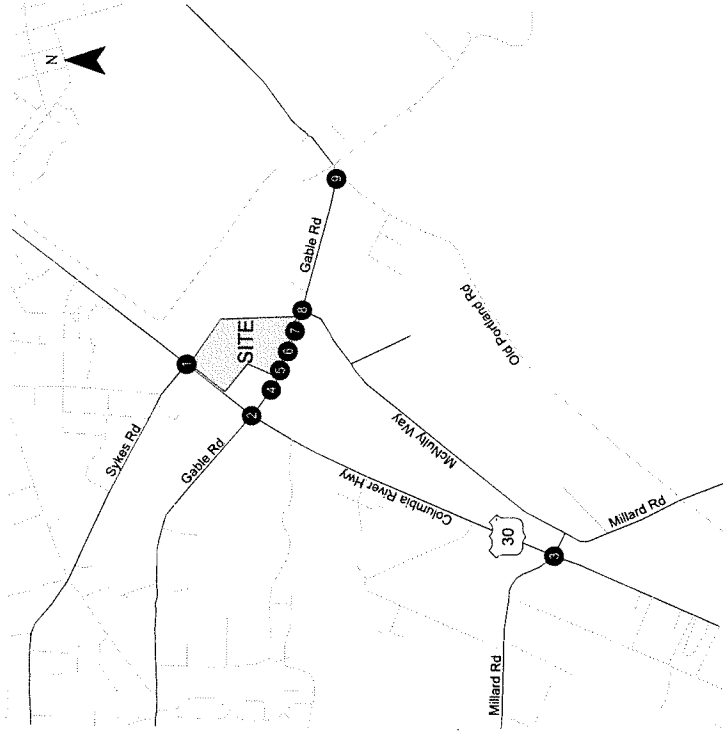


- STOP SIGN  
- TRAFFIC SIGNAL

Existing Lane Configurations & Traffic Control Devices  
St Helens, Oregon

Figure  
3





CM = CRITICAL MOVEMENT (UN SIGNALIZED)  
 LOS = CRITICAL INTERSECTION LEVEL OF SERVICE (SIGNALIZED)/CRITICAL MOVEMENT LEVEL OF SERVICE (UN SIGNALIZED)  
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Existing Traffic Conditions  
 Weekday AM & PM Peak Hours  
 St Helens, Oregon  
 Figure 4



Based on ODOT APM criteria, the McNulty Way/Gable Road intersection crash rate slightly exceeds the 90<sup>th</sup> percentile crash rate and is recommended for further review. However, as shown in Table 2, of the five crashes reported at the intersection over the five-year period, there is no reoccurring trend or repeating crash pattern in the data that would trigger possible safety mitigation associated with the proposed development plan.

**Table 2: Study Intersection Crash Summary (January 1, 2013 through December 31, 2017)**

Intersection	Crash Type					Crash Severity			Total	Crash Rates (Crashes /MEV)	ODOT 90 <sup>th</sup> Percentile Rate
	Rear-End	Turn	Angle	Ped/ Bike	Other	PDO <sup>1</sup>	Injury	Fatality			
US 30/Sykes Road	10	4				7	7		14	0.317	0.509
US 30/Gable Road	26	10	1		1	18	20		38	0.667	0.860
US 30/Millard Road	4	3	1		1	5	4		9	0.197	0.408
McNulty way/Gable Road	3	1			1	1	4		5	0.346	0.293
Old Portland Road/Gable Road	2				2	2	2		4	0.231	0.408

Source: ODOT  
MEV=Million Entering Vehicles

The ODOT Safety Priority Index System (SPIS) was also reviewed to identify potential sites along US 30 where safety issues warrant further investigation. The SPIS was developed by ODOT to identify hazardous sites on state highways through consideration of crash frequency, crash rate, and crash severity. Sites identified within the top 5 percent are investigated by ODOT staff and reported to the Federal Highway Administration (FHWA). Per the most recent SPIS list, no sites were identified in the top five percent of crash sites.

It should be noted that ODOT's All Roads Transportation Safety (ARTS) program is planning to install reflective back plates on the signal heads along US 30 to improve visibility and install advanced dilemma zone protection radar detection that can tell the speed of a vehicle on the approach to the intersection and adjust the green time accordingly.

## TRAFFIC IMPACT ANALYSIS

The traffic impact analysis identifies how the study area's transportation system will operate in the year the proposed development is expected to be fully built, 2021. The impact of traffic generated by the proposed development was examined as follows:

- Developments and transportation improvements planned in the site vicinity were identified and reviewed in coordination with City staff.
- Year 2021 background traffic conditions were analyzed at the study intersections during weekday AM and PM peak hours.
- Site-generated trips were estimated for the proposed development.



- A trip distribution pattern was developed, and the site-generated trips were distributed to the study area roadways and assigned to the study intersections.
- Year 2021 total traffic conditions were analyzed at the study intersections during the weekday AM and PM peak hours, assuming occupancy of the proposed development.
- On-site circulation issues and site-access operations were evaluated.

## YEAR 2021 BACKGROUND TRAFFIC CONDITIONS

The year 2021 background traffic conditions analysis identifies how the study area's transportation system will operate without the proposed development. This analysis includes traffic attributed to general growth in the region but does not include traffic from the proposed development.

### Planned Developments and Transportation Improvements

Kittelson identified and reviewed the planned developments and transportation improvements expected to be built within the study area prior to 2021. Per discussions with ODOT and City staff, four planned developments and one transportation improvement are expected to be complete within the study period.

#### *St Helens Medical Office Building*

The St Helens Medical Office Building (MOB) will include the construction of a 21,476 square-foot MOB in the southwest corner of the US 30/McBride Street intersection in St Helens. The planned development is expected to result in an increase in traffic volumes along US 30 and Gable Road.

#### *Emerald Meadows Estates Subdivision*

The Emerald Meadows Estates Subdivision will include the construction of a 78-lot subdivision located along Pittsburg Road in St Helens. The project site is located north of Helens Way, south of Pittsburg Road, east of Oak Ridge Street, and west of N Vernonia Road. The planned development is expected to result in an increase in traffic volumes along US 30.

#### *Red Leaf Subdivision*

The Red Leaf Subdivision will include the construction of 80 single-family attached dwellings located west of the intersection of Howard Street at Kelley Street in St Helens. The planned development is expected to result in an increase in traffic volumes along US 30.

#### *St Helens Apartments*

The St Helens Apartments will include the construction of 204 apartment units located south of McBride Street and east of Matzen Street in St Helens. The planned development is expected to result in an increase in traffic volumes along US 30 and Gable Road.



### ***US 30/Millard Road Traffic Signal***

ODOT is planning to install a traffic signal at the US 30/Millard Road intersection in 2019. The traffic signal will provide protected left-turn movement from US 30 to Millard Road and protected movements from Millard Road to US 30.

### **Traffic Volumes**

The background traffic growth rate used in this analysis to estimate increased traffic levels over the next 2 years was determined based on ODOT's Future Volume Tables and projected employment and population growth. Through traffic on US 30 is projected to increase by 41 percent over the 20-year planning period. Employment and population were expected to experience a 1.5 to 2 percent annual growth rate. As such, a two percent annual growth rate was applied to the 2019 traffic volumes to develop the year 2021 background traffic volumes. This growth rate was confirmed with City and ODOT staff during the scoping process. Figure 5 illustrates the resulting forecast year 2021 background traffic volumes during the weekday AM and PM peak hours.

### **Traffic Operations**

As shown, the background AM and PM peak hour operations are generally consistent with existing conditions. All intersections operate within the applicable standards and targets except the US 30/Gable Road intersection. *Appendix "D" contains the year 2021 background traffic conditions worksheets.*

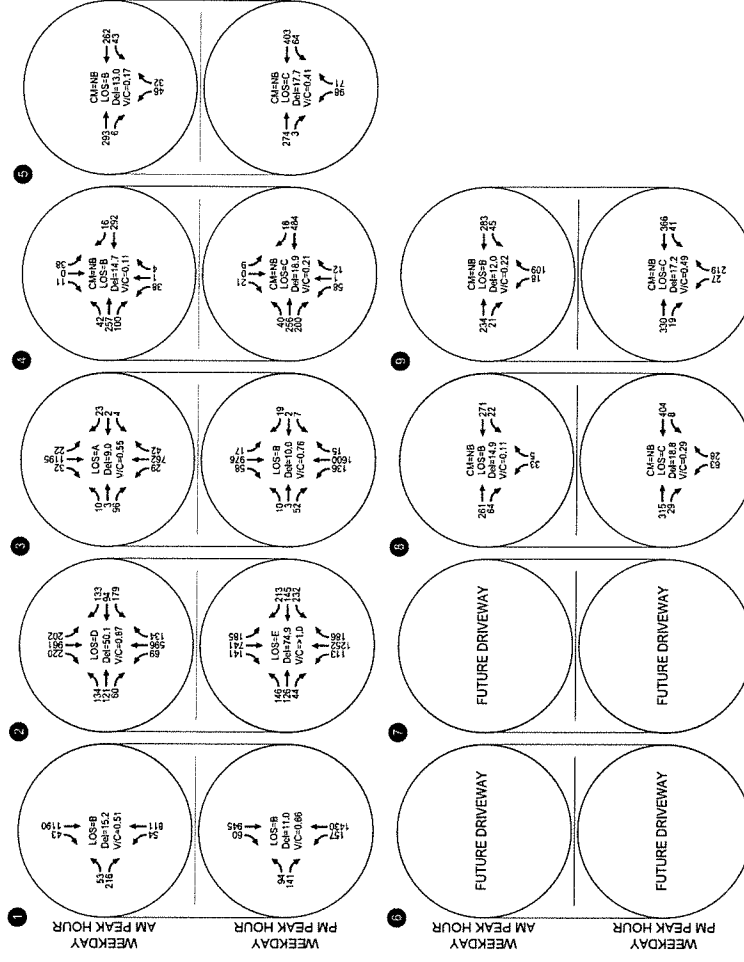
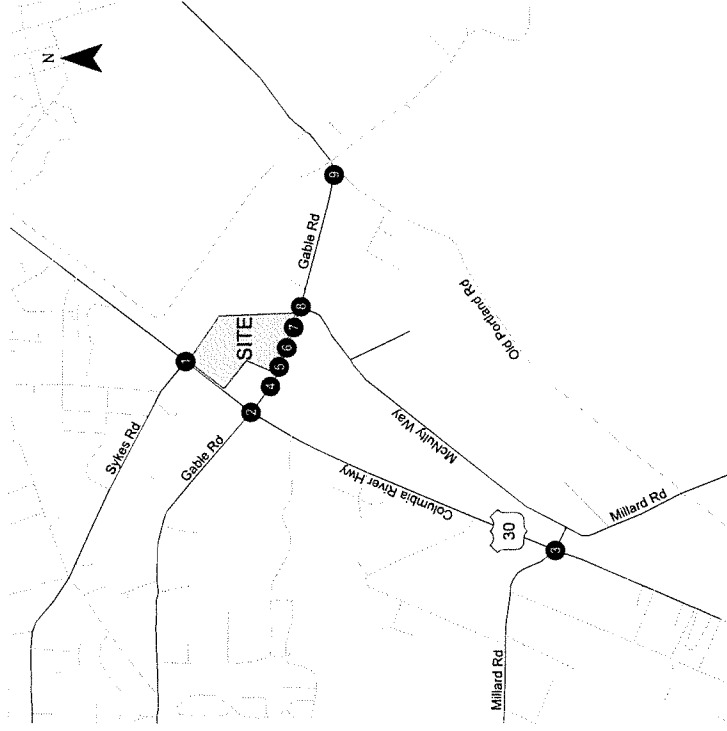
### ***US 30/Gable Road***

The US 30/Gable Road intersection is expected to exceed its applicable mobility target under year 2021 background traffic conditions during the weekday AM and PM peak hour. As previously noted, the TSP identifies a long-term (2022 to 2031) improvement at the intersection that includes the addition of a westbound right-turn lane. The City is currently collecting proportionate share contributions from developers for the improvement.

## **PROPOSED DEVELOPMENT PLAN**

The proposed development plan includes construction of 238 apartment units along with a clubhouse, playground, and other amenities, including 437 parking spaces. Access to the proposed development is planned to be provided via two new driveways located along the north side of Gable Road, approximately 660 feet and 990 feet east of US 30. One additional driveway is planned to be provided to the shared access road to the west of the site frontage for emergencies, pending an agreement with adjacent property owners. Construction of the proposed development is expected to begin in 2019 with full build-out and occupancy in 2021.





CM = CRITICAL MOVEMENT (UN SIGNALIZED)  
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Year 2021 Background Traffic Conditions  
 Weekday AM & PM Peak Hours  
 St Helens, Oregon



### Trip Generation

A trip generation estimate was prepared for the proposed development based on information provided in the standard reference manual, *Trip Generation, 10<sup>th</sup> Edition*, published by the Institute of Transportation Engineers (ITE – Reference 6). ITE land use code 220 (Low-Rise Apartments) was used as a basis for the estimate to provide a conservative analysis. Table 3 summarizes the estimates for the daily, weekday AM and weekday PM peak hours.

**Table 3: Trip Generation Estimate**

Land Use	ITE Code	Size (SF)	Daily Trips	Weekday AM Peak Hour Trips			Weekday PM Peak Hour Trips		
				Total	In	Out	Total	In	Out
Low-Rise Apartments	220	240 <sup>1</sup>	1,774	110	25	85	129	81	48

1. The trip generation estimate and traffic operations analysis reflect 240 units rather than 238 as planned to provide the developer with flexibility and to ensure a conservative analysis.

### Site Trip Distribution/Trip Assignment

The site-generated trips were distributed onto the study area roadways based on a review of existing traffic patterns and the location of major trip origins and destinations in the St. Helens and Columbia County area. Figure 6 illustrates the estimated trip distribution pattern for the proposed development. Figure 6 also illustrates the site-generated trips that are expected to use the study intersections during the weekday AM and PM peak hours.

## YEAR 2021 TOTAL TRAFFIC CONDITIONS

The year 2021 total traffic conditions analysis forecasts how the study area's transportation system will operate with the traffic generated by full build-out and occupancy of the proposed development. The year 2021 background traffic volumes shown in Figure 5 were added to the site-generated traffic shown in Figure 6 to arrive at the year 2021 total traffic volumes that are shown in Figure 7.

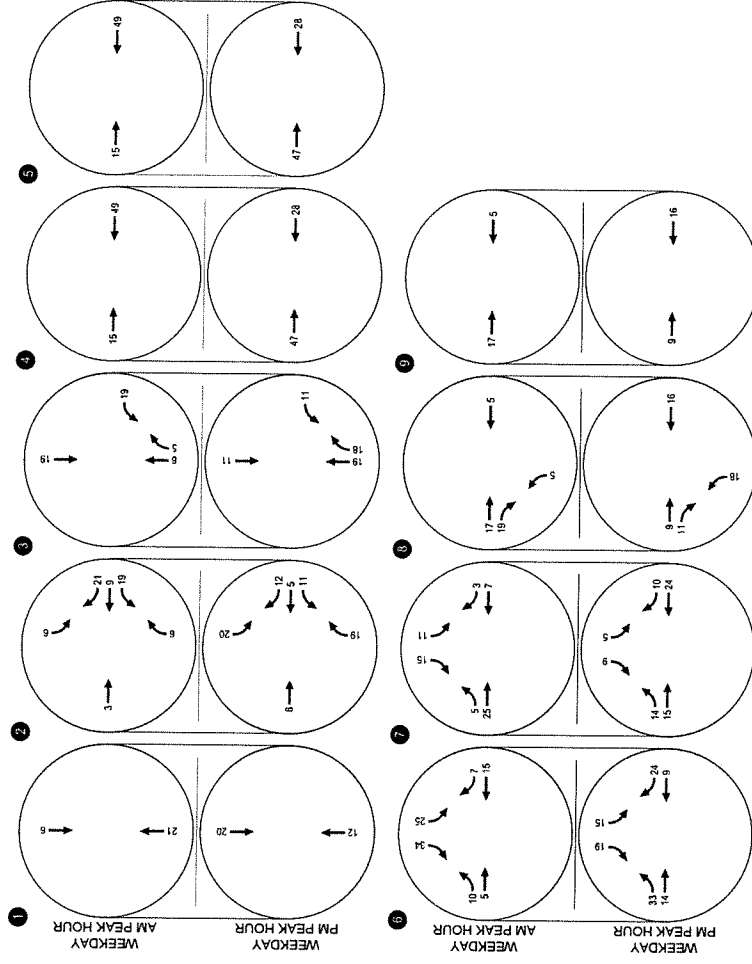
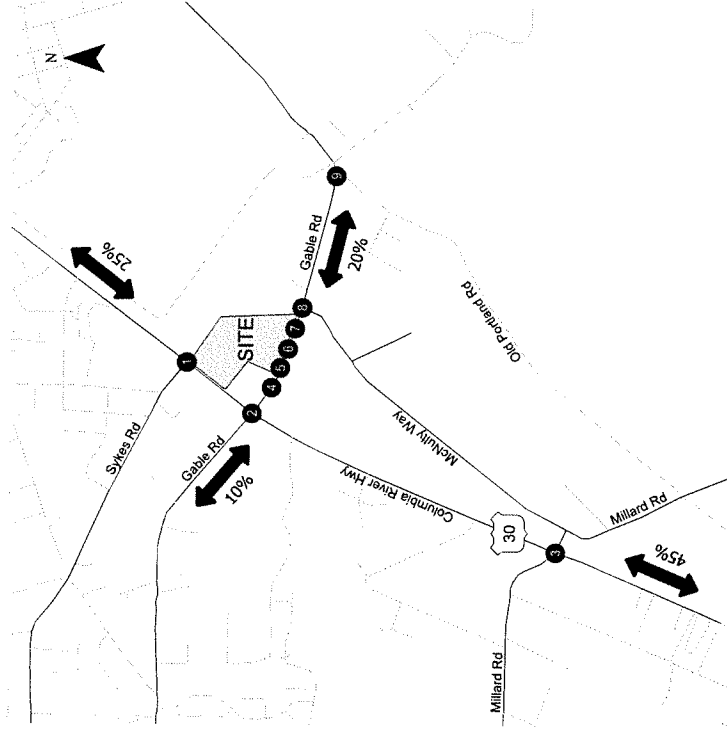
### Traffic Operations

The weekday AM and PM peak hour turning movement volumes shown in Figure 7 were used to conduct an operational analysis at the study intersections to determine year 2021 total traffic conditions. As shown, all the study intersections and site-access driveways are forecast to operate acceptably during the weekday AM and PM peak hours except the US 30/Gable Road intersection. *Appendix "E" contains the year 2021 total traffic conditions worksheets.*

### US 30/Gable Road

The US 30/Gable Road intersection is expected to exceed its applicable mobility target under year 2021 background traffic conditions during the weekday AM and PM peak hour. As previously noted, the TSP identifies a long-term (2022 to 2031) improvement at the intersection that includes the addition of a westbound right-turn lane. The City is currently collecting proportionate share contributions from developers for the improvement that reflect the impact of their developments.





Estimated Trip Distribution Pattern & Site-Generated Trips  
Weekday AM & PM Peak Hours  
St Helens, Oregon  
Figure 6



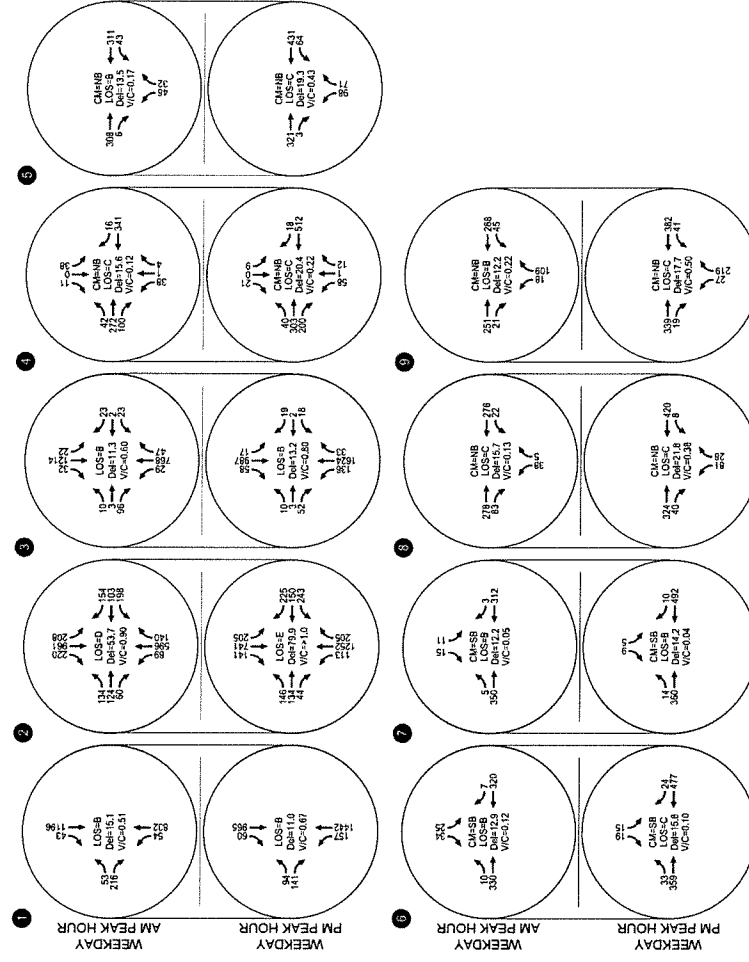
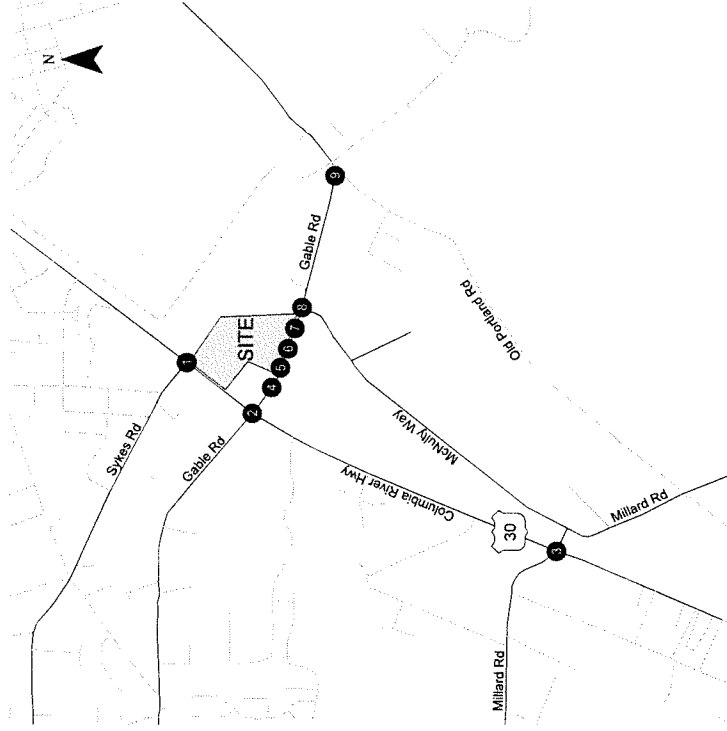
The proposed development is expected to increase the total entering volume (TEV) at the US 30/Gable Road intersection by approximately 2.2 percent during the weekday AM peak hour ( $64 / 2,903 = 0.022$ ) and by 2.1 percent ( $75 / 3,523 = 0.021$ ) during the weekday PM peak hour. It is recommended that the developer pay a proportionate share contribution that reflects the impact of the proposed development.

## ON-SITE CIRCULATION/SITE-ACCESS OPERATIONS

Figure 2 illustrates the proposed development plan. As shown, access to the proposed development is planned to be provided via two new driveways located along the north side of Gable Road, approximately 660 feet and 990 feet east of US 30. One additional driveway is planned to be provided to the shared access road to the west of the site frontage for emergencies, pending an agreement with adjacent property owners. Per the year 2021 total traffic analysis described above, both driveways are expected to operate acceptably with the proposed development during the weekday AM and PM peak hours. Vehicle queues at the driveways are expected to be less than one vehicle entering and exiting the site; the driveways are also not expected to be blocked by the queues associated with the US 30/Gable Road intersection or other driveways along Gable Road. Sight distance is also expected to be adequate at the proposed driveways.

It should be noted that while City code requires three driveways, given the size of the development, the traffic analysis shows that two driveways can provide access to the site while maintaining acceptable traffic operations. Also, frontage improvements along Gable Road will likely not provide sufficient pavement width to extend the existing center two-way turn lane further east. Therefore, the analysis does not account for possible two-stage left-turn movements at the site driveways. However, once properties to the south redevelop, or the City implements planned improvements along Gable Road, the presence of a center two-way left-turn lane would significantly improve operations at the site driveways. In addition, removing the dedicated left-turn lanes along Gable Road east of US 30 and extending the existing center two-way left-turn lane further west would similarly improve operations at adjacent driveways.





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Year 2021 Total Traffic Conditions  
 Weekday AM & PM Peak Hours  
 St Helens, Oregon

Figure  
 7



## CONCLUSIONS AND RECOMMENDATIONS

The results of this study indicate that the proposed St Helens Housing development can be constructed while maintaining acceptable traffic operations and safety at the study intersections, assuming provision of the recommended mitigation measures. The findings of this analysis and our recommendations are discussed below.

### FINDINGS

#### Existing Traffic Conditions

- All study intersections currently operate acceptably during the weekday AM and PM peak hours except the US 30/Gable Road and US 30/Millard Road intersections.
  - The TSP identifies a long-term (2022 to 2031) improvement at the intersection that includes the addition of a westbound right-turn lane.
  - ODOT is planning to install a traffic signal at the US 30/Millard Road intersection later this year.
- A review of historical crash data did not reveal any patterns or trends in the site vicinity that require mitigation associated with the proposed development.

#### Year 2021 Background Traffic Conditions

- All study intersections are forecast to operate acceptably during the weekday AM and PM peak hours except the US 30/Gable Road intersection. The intersection is expected to operate below capacity ( $v/c = 0.87$ ) during the AM peak hour and above capacity ( $v/c > 1.0$ ) during the PM peak hour.

#### Year 2021 Total Traffic Conditions

- All study intersections and site driveways are forecast to operate acceptably during the weekday AM and PM peak hours except the US 30/Gable Road intersection. The intersection is expected to operate below capacity ( $v/c = 0.90$ ) during the AM peak hour and above capacity ( $v/c > 1.0$ ) during the PM peak hour.

The City should continue to pursue a separate westbound right-turn lane at the US 30/Gable Road intersection to accommodate existing demand and anticipated long-term travel demand as identified in the City's TSP.

#### Site Access Operations

- All the site driveways are expected to operate acceptably under stop control.
- Vehicle queues at the driveways are expected to be less than one vehicle entering and exiting the site.



## RECOMMENDATIONS

- Pay a proportionate share contribution toward the planned improvements at the US 30/Gable Road intersection.
- Landscaping, above ground utilities, and signing should be located and maintained along the site frontage and throughout the site in a manner that preserves adequate intersection sight distance.

Regardless of the proposed development, for planning purposes, the City should consider removal of the dedicated left-turn lanes along Gable Road, east of US 30, and the installation of a continuous center left-turn lane to provide flexibility for movements related to all adjacent properties in the corridor.

## REFERENCES

1. City of St. Helens. *City of St. Helens Transportation System Plan*. September 2011.
2. Oregon Department of Transportation. *Oregon Highway Plan*. 2015.
3. Transportation Research Board. *Highway Capacity Manual, 6<sup>th</sup> Edition*. 2018.
4. Oregon Department of Transportation. *Analysis Procedures Manual*. January 2018.
5. City of St. Helens. *City of St. Helens Zoning Map*. 2016.
6. Institute of Transportation Engineers. *Trip Generation Manual, 10<sup>th</sup> Edition*. 2018.



# **CITY OF ST. HELENS PLANNING DEPARTMENT ACTIVITY REPORT**



**To:** City Council  
**From:** Jacob A. Graichen, AICP, City Planner

**Date:** 08.28.2019

This report does not indicate all *current planning* activities over the past report period. These are tasks, processing and administration of the Development Code which are a weekly if not daily responsibility. The Planning Commission agenda, available on the City's website, is a good indicator of *current planning* activities. The number of building permits issued is another good indicator as many require Development Code review prior to Building Official review.

## **PLANNING ADMINISTRATION—NOTEWORTHY ADMINISTRATIVE DECISIONS**

The Port and Mark Semling applied for a Lot Line Adjustment in May. Staff sent correspondence regarding some issues on June 6. Having not heard anything formal, we had to make a final decision this month. This was a denial based on work outside of approved areas from a County approved development in the 1990s and a condition of that County decision that no vegetation was to be removed within 50 feet of McNulty Creek. File LLA.7.19.

## **PLANNING ADMINISTRATION—PREAPPLICATIONS MEETINGS**

Conducted a pre-application meeting for a potential approximate 60 lot subdivision at the end of N. 8<sup>th</sup> and 9<sup>th</sup> Streets (North of Deer Island Road).

## **DEVELOPMENT CODE ENFORCEMENT**

Unpermitted sign observed at 2696 Columbia Boulevard earlier this year has been removed. Associate Planner spoke to the day care provider; issue took awhile to resolve but it was done civilly. *See photo to right →*



Worked with the developers of the property at the end of North 8<sup>th</sup> and 9<sup>th</sup> Streets (off Deer Island Road) get on the right track of protecting wetlands on that property. Some protected trees were removed during logging that occurred earlier this year. They applies for a Temporary Use Permit for a temporary sales office at the site and staff is requiring better flagging at the site to prevent any further unapproved impacts. This is the same property we had the pre-application meeting for described above.

## **PLANNING COMMISSION (& acting **HISTORIC LANDMARKS COMMISSION**)**

August 21, 2019 meeting (outcome): The Commission approved a Variance along N. 17<sup>th</sup> Street for a reduced front yard (setback) for a home and a Lot Line Adjustment at the Port of Columbia County's Multnomah Industrial Park. Note that the Lot Line Adjustment is a matter the Port discussed with the Council during visitor comments at the June 19<sup>th</sup> work session.

**As the Historic Landmarks Commission**, the commission recommended approval of a new sign on S. 1<sup>st</sup> Street as it pertains to the Riverfront District's architectural standards and scored applications for the city's next CLG grant funded Historic Preservation Rehabilitation Grant.



<https://www.ci.st-helens.or.us/planning/page/historic-preservation-rehabilitation-grant>

September 10, 2019 meeting (upcoming): Two public hearings are scheduled. One is for a residential fence Variance (height) along DuBois Lane. The other is for a Conditional Use Permit for a 238 unit apartment complex along Gable Road close to US30.

### **ST. HELENS INDUSTRIAL PARK WETLAND DELINIATION EFFORTS**

Reports sent to Army Corps of Engineers and Oregon Division of State Lands (DSL) in June for their review. Army Corps identified some clarifications needed for their review in July and staff is working with our wetland consultant on that. Still have heard nothing from DSL.

### **ST. HELENS RIVERFRONT CONNECTOR PLAN (TGM FILE NO. 2D-16)**

Bi-monthly cost match report (July / August) provided to ODOT for this grant. This should be the last one. This should also be the last time this project is in this report.

**ASSOCIATE PLANNER**—*In addition to routine tasks, the Associate Planner has been working on:*  
**See attached.**



## Jacob Graichen

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**From:** Jennifer Dimsho  
**Sent:** Tuesday, August 27, 2019 9:22 AM  
**To:** Jacob Graichen  
**Subject:** August Planning Department Report

Here are my additions to the August Planning Department Report.

### GRANTS

1. **Travel Oregon - Medium Grants Program (100k)** – Pole signs in fabrication in Eugene, OR by Ramsay Signs. Delivery to Public Works Shop still to be determined.
2. **OPRD – Veterans Memorial Grant** – Coordinated placement of additional monuments and final concrete pour/stormwater infrastructure with PW/VFW/Park staff. Shipment for remainder of granite slabs planned for installation in September.
3. **EPA – CWA Grant Program** – Conference call with County (as property owner) and consultants to discuss results of a Phase I/next steps. Planned for County Commissioner de-brief on 9/11 to discuss results. Submitted final Phase II Report for Semling property to EPA. EPA approval of supplemental sampling analysis plan for Semling Property for additional Phase II work. Additional sampling work scheduled for 9/30.
4. **CDBG- Columbia Pacific Food Bank Project** – First draw requirements submitted, 2nd quarter report submitted, 1st request for reimbursement submitted.
5. **Certified Local Government** – Project selected at August 13 meeting. Worked with Columbia Theater to approve work plan with the SHPO. Went through City's grant contract with applicant, preservation agreement, etc. Waiting on SHPO for notice to proceed.
6. **Safe Routes to School** – Prepared for quarterly report due 9/4/19.
7. **Housing Needs Analysis** – Closed out project. Final HNA document given to DLCD and posted online. Offered DLCD feedback via a survey on this program and project results.

### MISC

8. **Millard Road Park Property** – Worked with Mackenzie and staff to develop a Master Plan for the park. Worked to define Plan "A" and Plan "B" options which are bigger and smaller parks respectively.
9. **Parks & Trails Commission** – Reviewed and edited "Friends of" Group guidelines with staff. Reviewed and provided feedback on Dalton Lake presentation from sub-group, organized by Lynn.
10. **PSU Annual Housing Unit & Population Survey 2019** – Completed online survey for a deadline of August 15.
11. Began compiling planning-related data for **Police Station re-location** report for three potential sites
12. Registered for a Boardwalk Wetland Nature Park workshop in Milwaukie, OR on 9/18 to learn about wetland trail/boardwalk development
13. Union contract negotiations – Attended 2 meetings as a member of the bargaining team (outside of work hours)

### Jenny Dimsho

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